

April 10, 2008

MEMORANDUM

UTAH DEPARTMENT OF TRANSPORTATION

TO: Jim McMinimee, P.E., Chairman

FROM: Barry Axelrod
Recorder, Standards Committee

SUBJECT: Standards Committee Meeting Minutes and Next Meeting

The next meeting has been scheduled for Thursday, April 24, 2008 at 8:00 a.m., **in the 4th floor Project Development conference room of the Rampton Complex instead of the regular location.** No meeting was scheduled in December 2007 and the February 28, 2008 meeting was canceled.

Item	Remarks	Sponsor
1. Minutes of October 25, 2007	For approval	Barry Axelrod
2. Supplemental Specification, 01554M, Traffic Control	For approval (doc pg 30)	John Leonard
3. Supplemental Drawing, DD Series, Deletion for move to Manual of Instruction	For discussion (doc pg 49)	Robert Miles
4. Supplemental Drawings, DD 11A, Grade Separated Arterial	For approval (doc pg 62)	Robert Miles
5. Review of Assignment/Action Log	For review	Jim McMinimee
6. Meeting Improvements (on-going agenda item)	For discussion	Jim McMinimee
7. Other Business	For discussion	Jim McMinimee

JCM/ba

Attachments

cc:

Cory Pope Director, Region One	Stan Burns Engineering Services	Robert Miles Standards
Randy Park Director, Region Two	Richard Miller Bridge Design	Barry Axelrod Standards
David Nazare Director, Region Three	Greg Searle Construction	Patti Charles Standards
Dal Hawks Director, Region Four	Tim Biel Materials	Shana Lindsey Research
	Richard Clarke Maintenance	Tracy Conti Operations
	Robert Hull Traffic and Safety	Anthony Sarhan FHWA
	Michael Adams Traffic Management Division	Mont Wilson AGC
	Brad Humphreys Region 1, Preconstruction	Tyler Yorgason ACEC

October 25, 2007

A regular meeting of the Standards Committee convened at 8:00 am, Thursday, October 25, 2007, in the 1st floor conference room of the Rampton Complex.

Members Present:

Jim McMinimee	Project Development	Chairman
Robert Miles	Standards and Specifications	Secretary
Barry Axelrod	Standards and Specifications	Recorder
Stan Burns	Engineering Services	Member
Rex Harris	Region 1, Preconstruction	Member
Kris Peterson	Construction	Member
Richard Clarke	Maintenance	Member
Robert Hull	Traffic and Safety	Member
Tim Biel	Materials	Member
Boyd Wheeler	Bridge Design	Member
Michael Adams	TOC	Member
Mont Wilson	AGC	Advisory Member
Tyler Yorgason	ACEC	Advisory Member

Members Absent:

Randy Park	Region 2	Member
Anthony Sarhan	FHWA	Advisory Member

Staff:

Patti Charles	Standards and Specifications
Shana Lindsey	Research
Jerry Chaney	Environmental
Ray Cook	Bridge Design
Richard Hibbard	Traffic and Safety
Wes Starkenburg	Traffic and Safety
John Butterfield	Region 2 Materials
Bryan Lee	Materials
John Leonard	Traffic and Safety
Glenn Schulte	Traffic and Safety
Mike Donovan	Traffic and Safety
Paul West	Environmental
Terry Johnson	Environmental

Visitors:

None

Standards Committee Meeting

Minutes of the October 25, 2007 meeting:

1. Minutes of August 30, 2007 meeting were approved as written.

Motion: Robert Hull made a motion to accept the minutes as written. Seconded by Tim Biel. Passed unanimously.

2. Standard Specification 01355, Environmental Protection (Agenda Item 2) – Presented by Jerry Chaney.

Jerry said he needed to cover the two items from the previous meeting, Article 1.6 Hazardous Material Discovered During Construction and Article 1.7 Hazardous Material Contractor Caused. Jerry said he clarified the information to include more coordination with the Resident Engineer (RE). He said once hazardous material is discovered the RE is notified. Jerry went on to explain the requirements. He said the wording is better than before.

Jerry then discussed the contractor caused part. He said this includes a list of contractor actions when the contractor encounters or causes a spill of hazardous material. Jerry explained the requirements.

Jerry said this version includes the RE more than the previous version and that he believes this is adequate. He said those were the only major changes since the last meeting.

Discussion points were:

- Mont asked about payment. Jerry said there is a reference that work be performed as extra work and that was consistent throughout the section.
- There was no further discussion.

Motion: Boyd Wheeler made a motion to approve 2008 Standard Specification 01355 as presented. Seconded by Tim Biel. Passed unanimously.

3. Standard Specification 01456, Materials Dispute Resolution (Agenda Item 3) and Standard Specification Hot Mix Asphalt related (Agenda Item 5) – Presented by Tim Biel.

Tim said he would like to combine the items because they are related. He said they spent a lot of time, eight months to a year, with industry working through issues with 02741. He said this includes clarifying submittals, changing mix design requirements, defining items, and what constitutes the reverification. He said along the way they did some other things.

He said recycled asphalt was included in 02741 so Section 02969 can be deleted. He said as part of that the dispute resolution was removed and its own section created. Tim went on to discuss requirements with the specifications.

Tim said the updates to 02741 have gone through four or five reviews by the Pavement Council. He said all comments have been addressed.

Discussion points were:

- Barry commented that the agenda should have also included the deletion of Section 02743 based on his notes.
- Jim asked about longitudinal joint requirements and what was going to be done. Tim said they took a concept that Colorado DOT has been using for four or five years. He went on to explain the concept. Jim commented to Kris that the deputies had been working on something that came out of TRB, a rolling method for joints. Discussion continued on what will be taking place over the next couple of years and the cost - benefit.
- Jim said sometimes we get in quest of the perfect job and ignore the fact that it will cost a lot of money to approve something that has a really small business value. Tim said he didn't think this was one of those.
- Barry's comments on 02743 were addressed next. He said it was based on comments from Tim over the last several months of review for the 2008 book. Barry said he had it marked as tied to the approval of 02741. Barry added that it was not included on the agenda. Tim said all he could think of was it was something Karl had been working on. Barry said the submittal sheet did not cover 02743. Barry said if it is left in then it goes in the 2008 book as is because all his notes showed it as being deleted. John said he didn't remember any discussions on this section. Tim said he didn't remember anything related to deleting the section and that it is still a valid specification.
- Barry said he preferred separate motions because the discussion covered two agenda items.

Motion: Kris Peterson made a motion to approve 2008 Standard Specification 01456 as presented. Seconded by Robert Hull. Passed unanimously.

Motion: Kris Peterson then made a motion to approve 2008 Standard Specification 02741 and the deletion for 2008 of Standard Specification 02969 as presented. Seconded by Stan Burns. Passed unanimously.

4. Standard Specification 02735, Microsurfacing (Agenda Item 4) – Presented by Tim Biel and John Butterfield.

Tim said through the Pavement Council a couple committees put this together and after it was ready for the Standards Committee another change was recommended. Initially most of the changes were related to design and application. Tim said he was then asked to look at doing in-line testing so the item was not completed at the initial Standards Committee meeting.

Tim explained the request for the change. He said they spent a lot of time talking to the person and they couldn't come up with a resolution on how to set up testing, penalties, and what happens to the material on the road that is out of spec. He said they decided to use a Special Provision opportunity to deal with it and give themselves a few more months to work through it. He said a Special Provision wouldn't apply in all situations. He said everyone was fine with that direction.

Tim continued with the rest of the changes, asking John if he had anything specific to cover. John said the specification language was brought up to industry standards and that nothing else stands out.

Discussion points were:

- There was no significant discussion.

Motion: Kris Peterson made a motion to approve 2008 Standard Specification 02735 as presented. Seconded by Robert Hull. Passed unanimously.

Jim pointed out that item number 5 was previously covered and moved on to item 6.

5. Standard Specification 02752, Portland Cement Concrete Pavement (Agenda Item 6) – Presented by Tim Biel and Bryan Lee with assistance from John Butterfield.

John said this section has undergone format and unit changes, but this was the first time since 1979 that they took a wholesale look at our instructions to the contractor and our personnel based on today's working environment. He said this specification is a good example of that effort. John said a lot more direction was given in submittals.

He went on to highlight a few things. He said retesting for strength was revamped to be within industry standards. John said they were more attentive to who does what and that minimum sampling and testing was looked at for every section they deal with. He said another big change is texturing plans putting more responsibility on the designer instead of being locked into tining, allowing texturing options.

Discussion points were:

- Jim asked it that addressed noise issues and allowed designers more flexibility. John said designer can look at alternates for different settings.

John went on to discuss maturity testing in relation to the change to the specification.

Discussion points were:

- Jim asked how that was accomplished in the specification. He asked if this was just allowed and then the contractor would just choose. John covered the various options open to the contractor to choose from. Commenting that this may be too deep a question for Standards Committee, Jim asked about maturity being a measure of temperature and time put to a specific mix design. John agreed. Jim asked further related questions. John's response was maturity didn't fit into that.
- Commenting to Jim, Tim said he had highlighted one of the biggest problems in the concrete industry, that being that we don't have a good way to evaluate their ability short of putting it out on the road for 40 years and watching it. Jim said we have noted lower bag mixes in the process. Tim said lower bag mixes cure at a slower time and look better in the long run. Tim said the problem is that everything we have done in the last 5 years was to speed up the job and it is hard to stop that momentum. Jim then commented about pre-cast elements and what a lot of other states are doing for that exact same issue in both bridges and pavements. John provided comment on the subject.
- Boyd asked John about core locations listed in Article 1.5, paragraph C2c and the wording. Wording was discussed with "mid-way" being an option.
- Boyd said he had another question, on document page 133, Article 3.6, paragraph C1 dealing with tining depth on longitudinal tining and transverse tining. Boyd said in the first there is no depth but in the other it is approximate. John said this is a tough one in that they wanted something in there for their crews. Tim went on to explain the process and asked about adding the word "approximate."
- There was no further signification discussion.

Motion: Tim Biel made a motion to approve 2008 Standard Specification 02752 as discussed and modified. Seconded by Robert Hull. Passed unanimously.

6. Standard Specification 02789, Slurry Seal (Agenda Item 7) - Presented by Tim Biel and John Butterfield.

Tim said the same basic discussion we had on Agenda Item 4 for Microsurfacing applies here. He recapped the industry standards and Special Provision impacts related to the update.

Discussion points were:

- Jim asked about the significant difference between microsurfacing and slurry seal, commenting that it is the polymer asphalt that is used. Tim said that and the thickness. He said slurry is very thin and microsurfacing is $\frac{1}{2}$ to $\frac{3}{4}$ inch thick so stability is more important. Jim said the point of this comment was the mix design. Tim said slurry is more of a pre-mix whereas microsurfacing is a miniature version of HMA, therefore gradation is significantly more.
- There was no further discussion.

Motion: Kris Peterson made a motion to approve 2008 Standard Specification 02789 as presented. Seconded by Stan Burns. Passed unanimously.

7. Standard Drawing GW Series (GW 3 and GW 4) (Agenda Item 8) – Presented by Wes Starkenburg.

Wes said the change to GW 3 deals with the gutter pan and the issues that have come about on that. He said it needs to meet ADA requirements and that it is built correctly. For GW 4, Wes said that change is different. He said GW 4 was cluttered and difficult to understand. He said it was cleaned up and fixed to meet current practice. He said there was not a lot of change in product, just presentation.

Discussion points were:

- Jim commented that he liked the form they used for coordination comments and the fact there were a lot of comments.
- Someone pointed out there was no response to item #2 on the review form. Wes said they looked at that and had a comment related to it elsewhere on the form. Wes explained and that the spelling error was fixed.

Motion: Richard Clarke made a motion to approve 2008 Standard Drawings GW 3 and GW 4 as presented. Seconded by Robert Hull. Passed unanimously.

8. Standard Drawing SN 4, Flashing Stop Sign, Deletion (Agenda Item 9) – Presented by Wes Starkenburg.

Wes said when they went from wood to metal posts it required changes to a lot of drawings, including this one. The change to this drawing would require a complete revision. Wes said the drawing is not used very much and that the STOP Committee voted to delete the drawing. He said it wasn't a good use of effort to update the drawing and keep it current.

Discussion points were:

- Jim commented that it was a good business cost - benefit.
- There was no additional discussion.

Motion: Kris Peterson made a motion to approve the deletion for 2008 of Standard Drawing SN 4 as presented. Seconded by Tim Biel. Passed unanimously.

Agenda item 11 was covered next for another item from Wes. Barry suggested item 16 after that to complete all of Wes's items.

9. Standard Drawing ST Series (ST 1 and ST 5) (Agenda Item 11) – Presented by Wes Starkenburg.

Wes said the drawings were updated to include standard MUTCD signs. He said the other significant change was for the T-intersection signing. He said in some cases the current sign blocked people's yards. Wes said they created an urban version that is a shrunk down variety of the ones used on rural roads. He said that covered the changes.

Discussion points were:

- There was a comment if some updates were made. Wes said they were, after initially missing them.
- There was no significant discussion.

Motion: Kris Peterson made a motion to approve 2008 Standard Drawings ST 1 and ST 5 as presented. Seconded by Tim Biel. Passed unanimously.

10. Standard Drawings DD Series (Agenda Item 16) – Presented by Wes Starkenburg.

Wes said the current DD 5 drawing was drawn at such a small scale it was difficult to read. He said they split the drawing into two drawings. He said there were not a lot of content changes.

Discussion points were:

- Comments indicated that the details could still be increased in size. Wes agreed.
- Barry pointed out that DD 5 would be deleted with the approval of DD 5A and 5B. He said that part was not on the agenda.
- There was no further discussion.

Motion: Kris Peterson made a motion to approve 2008 Standard Drawings DD 5A and 5B as discussed and modified to increase the scale and for 2008 the deletion of Standard Drawing DD 5. Seconded by Tim Biel. Passed unanimously.

Jim then went back to the items by-passed and continued with agenda item 10.

11. Standard Specification and SN Standard Drawing Series for Traffic Signs (Agenda Item 10) – Presented by Glenn Schulte. (Seven separate submittals)

Glenn said he was given the assignment to review this set of drawings in July so he put a committee together for make the updates. He said the review and update was based on a request from Maintenance. He said he discussed this with Richard Clarke.

Submittal 1:

Glenn said the proposal is to eliminate wood posts. He said the change is related to the changes in the TC drawings. Glenn said that SN 8B was for temporary use by the contractor. He said this is a good solution for the contractor based on the TC drawing changes.

Discussion points were:

- Jim asked if there is a cost difference between types of posts. Glenn said it is difficult to determine cost - benefit. Glenn said they don't pay for posts, bases, or sign panels separately. He said they are paid in a unit. Glenn said later on he expressed a feeling that construction and designers would like this broken out into individual items. He said the reason is that for some bases we can use multiple posts. He said in some cases we are paying more for posts than required for the particular sign. Glenn said he tried to do a cost - benefit analysis but would have to look at each project to see what was done.
- Richard Clarke said the biggest benefit from the Maintenance side is when they repair the post. He said it costs more to put in a steel post than a wood post but when one gets knocked down it is usually just a matter of standing up the steel post. He said with a wood post the sign is destroyed and a new one has to be put in. He said from a Maintenance standpoint the cost - benefit is there. Richard said that is why they are pushing to go in that direction.

- The Committee decided to handle each item separately and vote on them that way.
- There was no further discussion on this item.
- Glenn pointed out that he received some technical updates to these drawings after they were submitted for the agenda, but were not significant.

Motion: Kris Peterson made a motion to approve 2008 Standard Drawings SN 8A and SN 8B and the deletion for 2008 of Standard Drawing SN 8 and SN 10 as presented. Seconded by Robert Hull. Passed unanimously.

Submittal 2:

Glenn said a post selection guide was created to assist in the selection of the proper post instead of contacting the manufacturer. Glenn said this was not specified in the past. He added that these were proprietary but not with this change. Glenn went on to discuss the drawings. He indicated the different systems are competing systems.

Glenn said there were earlier questions and discussion on strong versus weak soils. He said he will update the drawings to include that information.

Discussion points were:

- Tyler asked if as a designer do they have to evaluate each location to determine if it is weak or strong soil. Glenn said he didn't think so. He thought it might be more of a maintenance issue.
- There was no further significant discussion.

Motion: Kris Peterson made a motion to approve 2008 Standard Drawings SN 9A, SN 9B, and SN 9C as discussed and modified and the deletion for 2008 of Standard Drawing SN 9 as presented. Seconded by Tim Biel. Passed unanimously.

Submittal 3:

Glenn then moved on to SN 10. He said the current drawing, SN 11, is used by Maintenance, but it was hard to read. Glenn said the drawing was split into two drawings and renumbered. He went on to discuss the details of the drawings.

Discussion points were:

- There was no discussion.

Motion: Richard Clarke made a motion to approve 2008 Standard Drawings SN 10A and SN 10B and the deletion for 2008 of Standard Drawing SN 11 as presented. Seconded by Robert Hull. Passed unanimously.

Submittal 4:

Glenn said SN 11 is a new drawing. Glenn said this was something Maintenance had been using in the field. He went on to explain the details of SN 11. Glenn said proper installation information is included on the drawing because the base was not used properly in the past. He said if the base is going to be used then we should have a standard. Glenn said ASTM references needed to be added to the drawing.

Discussion points were:

- There was no discussion on SN 11.

Glenn moved on to SN 12. Glenn said this drawing formalizes what is being done by both contractors and maintenance. He said we never had a formal way of putting signs on concrete barrier and that this is not really anything new. Glenn pointed out a dimension change that came about after a discussion with manufacturers. He said the 3 $\frac{5}{8}$ inch dimension on the upper right corner of the two middle details needed to be changed to 4 inches.

Discussion points were:

- There was no discussion.

Motion: Richard Clarke made a motion to approve 2008 Standard Drawings SN 11 and SN 12 as discussed and modified. Seconded by Kris Peterson. Passed unanimously.

Submittal 5:

Glenn then moved on to the SN 13 series. He said they are all new drawings and that everyone had their own way of doing this. Glenn went on to explain the details of SN 13A.

Being no questions Glenn moved on to SN 13B. Glenn said this drawing details the hardware to be used. Again there were no questions.

Glenn moved on to SN 13C. He said the same thing was happening here with everyone doing it a different way. He said they looked at several Standards for Colorado and California to develop the charts. Glenn indicated "Pentagon" needed to be changed to "Pennant."

Discussion points were:

- There was no discussion.

Motion: Richard Clarke made a motion to approve 2008 Standard Drawings SN 13A, 13B, and SN 13C as discussed and modified. Seconded by Kris Peterson. Passed unanimously.

Submittal 6:

Glenn said the SN 14 series is the old SN 12 series. He said there was a lot of confusion using the old drawings and that he received a lot of calls for assistance. He said the table was updated with the help of Boyd Wheeler. Glenn said the post spacing requirements were clarified.

Being no questions on SN 14A, Glenn moved on to SN 14B. He said one inclined base was removed because it is seldom used in the State. He explained there are now one inclined base and one flat base. He pointed out that the manufacturer said the 3 inches can't be done so it was changed to 4 inches. He said all the systems are 350 approved.

Glenn moved on to SN 14C. He said the fuse plate on the old drawing pulled loose. He said this fuse plate is wind loaded and does not have torque requirements. He said other states use it and this plate is a better system.

Discussion points were:

- Barry pointed out the other SN 14 drawings have a note referring back to SN 14A for general notes but this one does not.
- There was some discussion on the 4 inch dimension versus a 3 inch dimension. Glenn said he would update the drawing as needed.

Glenn moved on to SN 14D. He said this is an updated version of the old SN 12C. Glenn said notes 6 and 7 need to be added. John explained the cost savings for sign fabrication. Glenn pointed out that the name of the drawing changed.

Glenn said SN 14E came from the old SN 12 drawings. He said the details were just cleaned up.

Discussion points were:

- In response to a comment Glenn said the 2005 version of the SN 12 series is deleted and replaced with this one. Glenn said there is a new SN 12 series. (SN 12 approved earlier.)

- There was no further discussion.

Motion: Stan Burns made a motion to approve 2008 Standard Drawings SN 14A, 14B, 14C, 14D, and SN 14E as presented and modified and the deletion for 2008 of the original Standard Drawing Series SN 12. Seconded by Robert Hull. Passed unanimously.

Submittal 7

Glenn discussed the M and P updates first. He said he has added or deleted items based on the approvals just completed. He went on to detail the additions and changes. Glenn said this gives UDOT a better way to track how much we are spending for sign panels, posts, and bases.

Discussion points were:

- Jim asked if this information is readily available for contractors and if this is how estimates are built. Someone said that is correct. Glenn said he talked to a lot of our designers on how information is called out. He said we now have standards on what the contractor is to put in.
- Referring to document page 221 Patti asked about the decision on using “PW” or “W.” Glenn said that has not been decided yet, but will be covered in the specification and how the Committee wants to handle it.

Glenn continued with Section 02891. Glenn said he took the old specification and added the parts on the brackets, signs, and posts and renumbered the signs based on previous discussions on the agenda.

Referring to document page 234, Article 1.4 paragraph D, Glenn said there has been a lot of comments and discussion on this to change the callout and use either “PW” or “W.” He said it is up to the Committee and he would then change the M and P to match.

Discussion points were:

- John asked Glenn to explain the confusion. Glenn said he could never get a straight answer as to why the confusion. John said there were a couple of issues. He went on to say before we were calling the poles P1, P2, P3, and P4. John said if you look for example at the signal specifications they call their signal poles the same thing. He said there were a lot of different areas where we were using the same thing. He said if we are using plywood why don’t we just call it PW so that we understand it is a substrate and not a location or type of pole. Barry said they had received comments on this over a number of years.

- Glenn asked the Committee for their direction. John said the “W” infers the type of post (W-beam) we use. He said PW is more of a reference to plywood. Glenn said he would change everything to PW.

Glenn continued with his discussion of the specification on document page 237. He said he left the entire part on pipes out, but has since added it to his file copy as paragraph E8.

Discussion points were:

- Barry asked about the soil definitions mentioned earlier and if they would be going in this section. Glenn said they would have to figure that out. Barry pointed out that Article 1.4 of all sections is now Definitions. Glenn said he would insert the definitions when he gets them from Geotech. Barry pointed out the deadlines.
- There was no further comment or discussion.

Motion: Richard Clarke made a motion to approve 2008 Standard Specification 02891 as discussed and modified. Seconded by Tim Biel. Passed unanimously.

Jim proposed a five minute break. The meeting continued after the break.

12. Standard Drawing SL 11, Traffic Loop Detector Details (Agenda Item 12) – Presented by Richard Hibbard.

Richard pointed out that the title on the agenda was incorrect and it should be Traffic Loop Detector Details. Richard said Larry Montoya requested that the drawing be updated to reflect current practice. Richard said this would eliminate problems with the drawing to include PVC loops. He said the PVC was assembled incorrectly and was leaking. He went on to explain the process. Richard said wire loops in asphalt also have problems because asphalt tends to be flexible so failures result.

Richard said recently they have done testing with preformed loops with good success. He explained the process, adding that this is a cheaper, easier, more controllable type of loop detection system. He said trenching loops into asphalt is a problem because of compaction and that potholing results. Richard said he didn’t think it was a good method. He said wire loops were left on as that method is still good.

Discussion points were:

- In response to a comment Richard said this change results in loops that last longer and don’t fail. He said it is also cost effective from a longevity and durability standpoint.

- Jim said he had heard a lot about not loop detectors, but other kinds of detectors like non-intrusive. Jim asked about the cost differential. Richard video detection is an option they have used but it tends to malfunction often. He said it is an easier way to go because you don't have to trench. Richard explained the video detection method and locations. He said radar detection isn't a proven method and is expensive.
- Robert Hull commented about a discussion at a recent Traffic Policy meeting with regard to video detection and about adding it into the signal design guidelines as to when video detection or loops can be used.
- There was no further significant discussion.

Motion: Kris Peterson made a motion to approve 2008 Standard Drawing SL 11 as presented. Seconded by Tim Biel. Passed unanimously.

13. Standard Drawing SL 18, Single Transformer Substation Details (Agenda Item 13) – Presented by Richard Hibbard.

Richard said the drawing needed to be updated as it was cluttered and contained information covered and defined already by NEMA, NEC, ANSI, and AASHTO. He said they decided to simplify the drawing to reflect more what our needs are on the job site beyond what is defined by those organizations.

Richard said they also allow for a prefabricated polymer concrete pad for the transformers. He said the drawing also reflects general items needed on the transformer. He said the primary power source details were eliminated because of conflicts with Rocky Mountain Power and other power companies.

Richard said the lighting pedestal is well defined in Section 16525 so the note on the drawing was removed.

Discussion points were:

- Jim asked about Clark's third comment on the Comment Form. He asked why pre-cast is not specified. Richard said the lighting pedestal has a base that has to be embedded in the concrete so it has to be cast-in-place. Jim commented about casting the plate in the concrete for use as pre-cast. Richard said no one ever proposed that but it could be an option.
- There was no further discussion.

Motion: Stan Burns made a motion to approve 2008 Standard Drawing SL 18 as discussed and modified to include the pre-cast option. Seconded by Kris Peterson. Passed unanimously.

14. Standard Specification 16525, Highway Lighting (Agenda Item 14) – Presented by Richard Hibbard.

Richard said the section was updated for currency with industry and regulatory standards. He said they had 62 comments to consider during the final review.

Discussion points were:

- Jim commented that he liked the comment form and congratulated Traffic and Safety for use of the form. Barry said a version taken from their form is now available on the Web site for use by all areas.

Richard said a vast majority of the comments were from Structures and steel suppliers with regard to which ASTM Standard to use. He said they also had several comments from lighting suppliers.

Richard said this change was prompted by Larry Montoya's desire to get the pedestal specification more defined. He said changes were also made to their procurement specification.

Richard commented that he changed a lot of the information for poles. He detailed some of the updates to meet current industry standards and what was deleted. Richard said under lamps they removed items that had little bearing on specifications. He said the Underground Service Pedestal portion was completely replaced by the Lighting Power Pedestal.

Richard said that covered the majority of the changes and the rest were just to get up to industry standards. He went on to say he added a couple of items that should prove cost effective from a maintenance standpoint. He detailed those.

Discussion points were:

- Tim asked why there is a reference to Hot Mix Asphalt in article 2.16. He said he wasn't sure how it applied. Richard said he thought it would apply whenever there is any saw cutting for conduit trenching. Discussion continued on the wording and asphalt reference. Richard added that he also thought it was there for use related to backfill if a light pole had to be drilled out. Richard said it was this way in the original specification and because he wasn't sure why and couldn't come up with a reason to take it out he left it.
- If reference to article 3.12 someone asked what a "megger meter" was. Barry said that has been in the standard for years. Discussion continued on the correct wording and usage. Richard will check and update as needed in the final version.

Motion: Stan Burns made a motion to approve 2008 Standard Specification 16525 as discussed and modified. Seconded by Robert Hull. Passed unanimously.

15. Standard Drawing TC 5, Work Zone Business Access Signing (Agenda Item 15) – Presented by John Leonard.

John said last time they deleted TC 5 as an extraneous drawing. He said they have had requests for a drawing to standardize signs for business access. He said currently there are many different methods being used. John said this new drawing proposes a standardized method so contractors know what they are dealing with. He said they received a couple of comments, one of which dealt with adjusting the spacing for different speeds of roads. John indicated that was added in as was a note for right side optional signing.

Discussion points were:

- Stan asked John to explain Note 6. John said the detail just below the notes was added based on the comments for opposite side signing. He said drivers usually look on the side of the road where the access is located, not where you would look for traditional signing. John said the requirement was made optional and the Resident Engineer would provide additional information when needed. John said they didn't think it was an essential sign. John said it is an optional sign if wanted on the opposing side.
- John said there is nothing different they are doing here other than standardizing where the sign is placed and the color scheme.
- Someone asked if the wording of Note 6 needed to be fixed because currently it isn't very clear. Mounting was also discussed. John said mounting has to be NCHRP compliant. There was a comment to add that to the notes as well. John said they don't state that, explaining that it is consistent with how they do it on other drawings. John said there are different mounting methods but they don't specify them because the contractor is obligated to follow NCHRP guidelines.
- There was another question to clarify Note 6. John said the note is going to be rewritten. He went on to further explain the note.

Motion: Stan Burns made a motion to approve 2008 Standard Drawing TC 5 as discussed and modified. Seconded by Robert Hull. Passed unanimously.

16. Standard Specifications, Structure Concrete related (Agenda Item 17) – Presented by Ray Cook.

Ray said these are two new specifications, Dampproofing and Concrete Coating. He said the Dampproofing specification was used on the Legacy Project as a Special Provision. He said it would be applied when called out on the plans.

Discussion points were:

- Jim said he was trying to understand why it is needed on structures. Boyd said it is not absolutely necessary but was an enhancement on the Legacy Project. Some discussion continued.
- Stan asked about a cost - benefit ratio and if that had been done. Ray said no.
- Richard Clarke said most are using it to prevent salt from getting into the steel. He said he didn't know how important that is, but that is a long term benefit. Boyd agreed.
- Stan suggested Ray come back and provide more information before approving. Barry pointed out that if not approved then they need to go back and modify a section approved at the last meeting that referred to Section 07111. Richard commented that we are not making this a requirement, just if you decide to then the specification is provided.
- Boyd suggested the section be approved for use on projects and then just go with what is specified on the plans.
- There was no additional significant discussion.

Motion: Boyd Wheeler made a motion to approve 2008 Standard Specification 07111 as discussed and modified. Seconded by Kris Peterson. Passed unanimously.

Ray covered Section 09981, Concrete Coating next. He said this section was used on the I-15 Reconstruction Project, the Legacy Project, and other UDOT projects. He said it is for staining concrete. He indicated they used the Special Provision and updated it for this Standard. He went on to explain usage and that it is as specified for the project.

Discussion points were:

- There was a comment and question with respect to Article 3.2 paragraph E with respect to the "Chlor*Rid" reference. Parts of the conversation were too soft and contained considerable background noise of paper shuffling or other noise. Ray said it was the only one he could find. He said that was what was specified in the proprietary specification and he tried to find something else. He didn't know if there were any other products to remove the salt. Boyd said from their perspective they aren't telling them they have to use Chlor*Rid. Comment was to indicate the requirement without reference to a product.
- Barry said it is already referenced in the main E paragraph with the text "or approved equal" so the "such as" text in E1 isn't needed.

- Someone asked why do we need to prescribe the method, adding that if the level is above a certain number get it below and let the contractor figure it out. Richard Clarke asked if the contractors even know there is such a thing out there. Boyd said they can take it out.
- Robert Hull asked how many times do we specify a company's brand in our specifications whether there is one more.
- Barry suggested just state "Perform chloride testing." He said the rest of the information isn't needed. Boyd said two different company tests are listed. Side discussions continued. Comment indicated the two tests might be our approved method.

Motion: Kris Peterson made a motion to approve 2008 Standard Specification 09981 as discussed and modified to remove the Chlor*Rid reference from paragraph E1. Seconded by Tim Biel. Passed unanimously.

17. Standard Specification and Standard Drawings Wildlife Crossed Related (Agenda Item 18) – Presented by Paul West.

Paul said that he is asking to change the titles of the Standard Drawings to something a little more meaningful. Paul suggested the name be Wildlife Escape Ramps to better clarify the purpose of the structures. He said in the process of coordinating the name change he received a lot of comments suggesting other changes to the drawings and updates to the specification. He said they are all minor changes.

Discussion points were:

- Jim commented that there were a lot of comments on the log. Paul said the changes were minor and editorial in nature.
- There was no discussion.

Motion: Richard Clarke made a motion to approve 2008 Standard Drawings FG 4A and FG 4B as presented. Seconded by Kris Peterson.

Discussion points were:

- Mont asked how the ramps would be paid for. Jim commented that was related to item 27 on the comment log and asked Paul to respond. Jim read the response from the comment log and said he thought it would be lump sum. Paul said he assumed so, asking how were they paid before. He said that wasn't anything that changed and that is beyond what he was trying to do. Jim asked if anyone else had an answer. Discussion continued but it was difficult to understand. No change or update was indicated.

Motion: Being no further discussion, Jim called the question. Passed unanimously.

18. Standard Specification 02936, Vegetation Establishment Period (Deletion) (Agenda Item 19) – Presented by Terry Johnson.

Terry said the specification covers general requirements and has to be modified every time it is called for in a project. He said at times there has been confusion as to applicability for certain items. He referred to the submittal sheet for six items related to the recommendation.

Discussion points were:

- There was a question as to any direction in specifications in lieu of this, commenting that something may be left out. Terry said you have to take different items depending on what is being put on a project. A follow up question asked if there could be a detail or lead sheet on what is being established. Terry said he didn't follow what was being said. The discussion was again difficult to understand as most comments were too soft. This didn't seem to be significant as there were no recommendations or other comments.

Motion: Robert Hull made a motion to approve for 2008 the deletion of Standard Specification 02936 as presented. Seconded by Kris Peterson. Passed unanimously.

19. Sub-Committee Update on other Standards Approvals (Agenda Item 20) – Presented by Robert Miles and Barry Axelrod.

Robert said they discussed several items in their meetings to include public information and erosion control. Barry said they had several individual meetings with Wes and Richard on Traffic and Safety drawings going over a lot of their drawings. Barry said as a result of those discussions there were a lot of editorial changes with some changes resulting in items on today's agenda. Barry said some changes were subcommittee approved.

Discussion points were:

- In response to a comment from Robert Miles, Barry said the Public Information specification didn't have to come to the Standards Committee and that they were still waiting for updates from the Public Information Officers on the specification. Barry said there were some wording changes and rearranging of information and that he was still waiting to get that information.
- Robert thanked everyone for their efforts in getting the 2008 Standards together. He discussed the publishing dates, adding that he didn't see a problem with launching the implementation on time for January 1. Barry said the drawing deadline to get to printing isn't as critical as the specifications because the specifications are being printed outside the state while the drawings are being done at State Printing. Barry said they didn't need to get all the drawing books up front and could get reprints as needed.

- Boyd commented about a couple of design-build projects that would need information in November. Barry said he as already been contacted by a couple of companies and has that worked out, explaining what was worked out.
- There was a question as to whether we had some to the same issues as when the 2005 Standards first came out. Barry said one of the questions was what to use after January 1 for projects still being worked on. Barry said at one time they went two or three years using two different versions, trying to maintain two different sets of specifications. Barry said that was ridiculous and very difficult to do. Barry said the 2008 Edition will be issued with a Priority 3 and everything after February 1 has to use the 2008 Edition unless a request for a waiver is sent to Robert. The waiver would be for an additional 30 days. Barry said this would then not put us in the same position as in the past where after several months we are still trying to use the old Standards.
- Barry then reviewed his listing of specifications and drawings, pointing out various discussions and open or problem items. Barry said he resolved a lot of red items in the Construction area with Kris. Barry said there were a couple of hydraulics issues but those are being worked with Jim Baird. Barry continued reviewing the listings. Barry said everyone was aware of the deadlines for this meeting. Barry said based on the reviews by Structures and Traffic and Safety if the specification or drawing hasn't come to the Committee by now then they are good with their items. There was no disagreement. There were a couple of items that needed a follow-up. Barry said he didn't think any significant items have been missed.

20. Review of Assignment/Action Log (Agenda Item 21)

Jim said it didn't look like much was left on the Action Log, asking Barry to cover the Log.

- Item 1, Supplemental Specification 01554. Barry said he still wasn't sure what was happening with this one that has been open for a couple of years. He said something might be walked through on this, asking John for an update. John said Robert Miles was given some information to discuss with the group. Robert said that has not been done yet.
- Item 2, Standard Drawings BA 4E, W-Beam Guardrail Installations and ST 8, Plowable Pavement Markers. Barry said this one has been on the log for a while but it looks like there is no impact and nothing related to this has come up in any of the other reviews. Barry recommended closing this one. John said he would check with Glenn and Mike and if there is nothing then it can be closed. No subsequent information was received. Item closed.
- Item 3, Supplemental Specification 02735, Micro-Surfacing. Barry said items 3, 4, and 5 were approved today as 2008 Standard Specifications. Closed.

- Item 4, Supplemental Specification 02789, Asphalt Slurry Seal Coat. Approved as a 2008 Standard Specification. Closed.
- Item 5, Standard Specification 01355, Environmental Protection. Approved as a 2008 Standard Specification. Closed.
- Item 6, SW Standard Drawings. Barry said this is the cracking issue. Boyd said that has not been resolved and is still open. Boyd said as of now he hasn't been able to determine how big an issue it is. Barry asked if we are alright with what has been approved. Boyd said it may generate something to the new specifications.
- No new items were added to the action log.
- The status report as handed out at the October 2007 meeting follows:

Action Item Update for October 25, 2007 Standards Committee Meeting

(As of October 11, 2007, 10:00 a.m.)

Item 1, Supplemental Specification 01554M, Traffic Control: John Leonard is walking this through the Standards Committee for approval of recommended actions. No other information at the time of publication of the minutes package.

Item 2, Standard Drawings BA 4E, W-Beam Guardrail Installations and ST 8, Plowable Pavement Markers: Glenn Schulte is checking into this to see if the drawings are impacted by the deletion of Sections 02762, Plowable Pavement Markers and 02773, Asphalt Concrete Curt. No other information at the time of publication of the minutes package.

Item 3, Supplemental Specification 02735, Micro-Surfacing. On October agenda as a Standard Specification change for 2008.

Item 4, Supplemental Specification 02789, Asphalt Slurry Seal Coat. On October agenda as a Standard Specification change for 2008.

Item 5, Standard Specification 01355, Environmental Protection. On October agenda as a Standard Specification change for 2008.

Item 6, SW Standard Drawings, cracking issue. Boyd said he talked to Materials and a cracking issue was identified. This will require a future drawing change but it is not ready at this time. The drawings as approved at the August meeting will be published as is. Additional information to be provided by Boyd at the October meeting.

21. Meeting Improvements (on-going agenda item) (Agenda Item 22): Jim said today was a testament on how we can get through a lot of items is we are organized and prepared. Jim said the process looks to be working very well. He thanked everyone.

22. Other Business:

Kris said he had an item that didn't make it on the regular agenda. Kris said the AGC proposed using Prompt Payment on all projects, not just Federal Aid projects. He said the reasoning is two-fold. He said project closeout is a problem because of subcontractor closeout. He said they could be long gone and have no interest in helping out. He said the change to Section 01284, Prompt Payment, was to have it apply to all projects. There was no other change to the Section.

Kris said comments from the contractors indicated the benefits outweigh the negatives.

Discussion points were:

- Barry, referring to the current Supplemental Specification, said if approved there would also be a change to Section 01282, Payment, Article 1.1 paragraph E to remove the parenthetical reference to Federal-Aid Projects. Barry said Article 1.9. paragraph E1 refers to Federal-Aid Projects so that would have to be corrected. He said the paragraph numbering would also have to be corrected.
- In response to a comment from Jim, Kris said currently there is a state law that says this has to be done within 30 days, adding there is nothing to motivate that. Kris added that they are hesitant to do anything not mandated by the Feds on any job. He said they haven't pushed anything in this direction until the contractors came to them. Kris said he was sure some comments would come to them after they implement the change. He said this doesn't make the contractor do any extra work, it just makes them do it sooner.
- Mont said he concurred with Kris. He said the benefit is that it would accelerate the final closeout. Mont said the bottom line is that he is for the change.
- Jim asked Barry to explain the process because this is coming up under Other Business and having not gone through the regular process other than it looks like Kris did a good job going through the notification process. (A submittal sheet was included in the handout.) Jim said the timeframes may not have been met. Barry said the only thing is the Committee hasn't had the review time of two weeks to look at the change. Barry said the submittal sheet covers everything else. Barry said approval would have to be like it was several years ago, before the submittal sheet process, where a two-part motion is needed. He said the first would be to waive the process and the second to approve the item. Barry asked about the priority on the submittal sheet, saying that it showed a priority 2, but this would be for 2008 and therefore a priority 4.

Motion: Robert Hull made a motion to waive the rules temporarily to vote on this item. Seconded by Tim Biel. Passed unanimously.

Motion: Tim Biel made a motion to approve 2008 Standard Specification 01284 as discussed and modified and with the required change to Standard Specification 01282. Seconded by Stan Burns. Passed unanimously.

A motion was made, seconded, and approved to adjourn.

The next regular meeting of the Standards Committee has been scheduled for Thursday, April 24, 2008, at 8:00 a.m., in the 1st floor conference room of the Rampton Complex. The scheduled February 28, 2008 meeting was canceled because there were no agenda items.

Approval of Minutes: The foregoing minutes were approved at a meeting of the Standards Committee held _____, 2008.

Assignment/Action Item Log

Date Initiated/Updated	Item #	Action	Assignments	Status	Target Date
August 25, 2005	1	- Supplemental Specification 01554, Traffic Control (originally tracked as 00555M, Prosecution and Progress, Limits of Operation): Coordinate the required action to have the process placed in the proper location, to the detail necessary and bring the recommendation to the Standards Committee for approval.	John Leonard	Open	April 2008 meeting.
October 27, 2005		- Item not ready. To be reviewed by the Operations Engineer. Target date updated.	Tracy Conti		
February 23, 2006		- Direction being reviewed by upper management.	Robert Hull		
April 27, 2006		- Still being review by upper management for direction.			
June 29, 2006		- No change other than item may be on hold.			
August 31, 2006		- No change.	Robert Hull		
November 30, 2006		- Item being reviewed. Changed to track as Section 01554.			
February 22, 2007		- Still being worked			
April 26, 2007		- This item was incorporated at the request of the Standards Committee into the Traffic Spec 01554. This will be done in the review and modifications to this spec, before the August deadline			
June 28, 2007		- No new information. Not due until August.			
August 30, 2007		- No new information. Past due.			
October 25, 2007		- Information passed on but not finalized.			

Date Initiated/Updated	Item #	Action	Assignments	Status	Target Date
August 30, 2007	2	- SW Standard Drawings. Research column cracking problem and if needed update the drawings per agenda item 11 from August 30, 2007 meeting.	Boyd Wheeler	Open	April 2008 meeting.
October 25, 2007		- Not resolved. Not sure how big an issue. May require future change.	Contact changed to Richard Miller at later time due to personnel changes.		

Closed Items From Last Meeting (August 30, 2007)					
Date Initiated/Updated	Prior Item #	Action	Assignments	Status	Target Date
April 26, 2007 June 28, 2007 August 30, 2007 October 25, 2007	2	- Standard Drawings BA 4E, W-Beam Guardrail Installations and ST 8, Plowable Pavement Markers to be looked at for updates related to the deletion of sections 02762, Plowable Pavement Markers and 02773, Asphalt Concrete Curb. For inclusion in 2008 version. - No new information. Not due yet. - No new information. - John checked. No update required.	Robert Hull Mike Donovan (BA 4E) John Leonard (ST 8)	Closed	Closed
June 28, 2007 August 30, 2007 October 25, 2007	3	- Supplemental Specification 02735, Micro-Surfacing. Update Sections 02735 and 02789 to meet sampling requirements. - Updated specification needed. - On agenda. Approved.	Tim Biel	Closed	Closed
June 28, 2007 August 30, 2007 October 25, 2007	4	- Supplemental Specification 02789, Asphalt Slurry Seal Coat. Tied to item 4. - Updated specification needed. - On agenda. Approved.	Tim Biel	Closed	Closed
August 30, 2007 October 25, 2007	5	Standard Specification 01355, Environmental Protection. Review meeting comments and update specification accordingly. - On agenda. Approved.	Jerry Chaney	Closed	Closed

Standards Committee Agenda Items Section

Submittal Sheets, Supplemental Specification Drafts, Standard Drawing Drafts, and other supporting data for the April 24, 2008 Standards Committee meeting follows.

Standards Committee Submittal Sheet

Name of preparer: John Leonard
Title/Position of preparer: Operations Engineer
Specification/Drawing/Item Title: Traffic Control
Specification/Drawing Number: 01554

Enter appropriate priority level:

(See last page for explanation) 3

Sheet not required on editorial or minor changes to standards. Check with Standards Section.

NOTES:

1. All Submittal Sheets must be completed and sent to the Standards Section by the Standards Committee suspense date as shown on the Web.
(<http://www.udot.utah.gov/go/standardscommittee>)
2. The Preparer of the Submittal Sheet or the Standards Committee member (or authorized substitute) responsible for the submittal must be present at the Standards Committee meeting and capable of discussing and answering all questions related to the submittal. The item will be postponed to a later meeting if one of these people is not present.
3. Notify the Standards Section immediately of any changes that impact the presentation to include absence of sponsor or delay in presentation.

Complete the following: (Use additional pages as needed.)

- A. Why? Detail the reason for changing the Standard (Specification or Drawing), what has initiated a new Standard, or what has caused a new or changed item of interest.

1. **Three editorial changes are proposed to bring the Specification into conformance with the MUTCD and the Standard Drawing TC Series**
 - a. **Clarification on the use of safety apparel for flaggers.**
 - b. **Conformance with TC drawings allowing additional use of directional indicator barricades and requiring the use of barrels or indicator barricades for tapers only for speeds of 50 mph and above (previously required all devices to be barrels/indicator barricades—now only the taper is required).**
 - c. **Conformance with TC Drawings—use term *Work Zone*, not *Construction Zone*.**
2. **There have been instances where contractor personnel, without the knowledge of the Department or local law enforcement, have performed a slow down on the interstate. Some of these have resulted in crashes. Senior management has requested a supplemental specification be created to address this issue.**

B. Measurement, Payment, Acceptance, and Documentation:

1. How is Measurement and Payment handled? Existing (from the measurement and payment document), modified, or new measurement and payment to be included with all Standard Specifications or Supplemental Specifications.

There is no change to measurement and payment. This supplemental requires notification and a set procedure for performing a slow down.

2. How is Acceptance and Documentation handled? Existing (from the acceptance and documentation document), modified, or new acceptance and documentation to be included with all Standard Specifications or Supplemental Specifications. Include Contractor Submittals, Inspection Elements, and Documentation.

No change.

C. Stakeholder Notification for AGC and ACEC:

By email provide the AGC and ACEC Standards Committee member a copy of all pertinent information relating to the specification or drawing. Detail all responses below. Indicate if no comments were received.

Note: There is a two-week response time set for this item.

Refer to the Standards Committee Web site, Members page at <http://www.udot.utah.gov/main/f?p=100:pg::::1:T,V:659> for the respective e-mail addresses.

AGC Comments: (Use as much space as necessary.)

See attached comment resolution sheet.

ACEC Comments: (Use as much space as necessary.)

See attached comment resolution sheet.

D. Stakeholders? From the list provided, document the stakeholders contacted, detailing: the company, name of contact, how contacted (by phone, email, hard copy, or in person), concerns, and comments of the change. Stakeholders:

Note: There is a two-week response time set for this item. Allow Stakeholders two weeks to process and respond to coordination requests. All areas should try to complete review and comment as soon as possible but within two weeks.

In-house (for example, preconstruction, materials, construction, safety, design, maintenance) (Include all applicable in-house areas even if not listed above.)

See attached distribution and comment resolution sheets.

Construction Engineers

Contractors (Any additional contacts beyond “C” above.)

Suppliers

Consultants (as required) (Any additional contacts beyond “C” above.)

FHWA (To be accomplished as part of the two-week process before submitting to the Standards for inclusion on the Standards Committee agenda.) (This is in addition to the requirements of UDOT Policy 08A5-1, procedure 08A5-1.3.)

Others (as appropriate)

E. Other impacted areas, systems, or personnel. (Consider all impacts and possible changes to these areas during the preparation process. Coordinate with all appropriate areas for the respective item. List all impacts and action taken.)

1. Minimum Sampling and Testing Requirements

None.

2. Business Systems (Electronic Bid System, Project Development Business System, Electronic Program Management, Computer-Aided Drafting and Design, etc.)

None.

3. Implementation Plan (Provide detailed instructions on how the subject item will be implemented to include notification of all interested parties and training requirements.)

Notification will be provided through this Specification and regular training opportunities to field personnel.

F. Costs? (Estimates are acceptable.)

1. Additional costs to average bid item price.

Possible increase to lump sum cost of traffic control if reimbursement is required for law enforcement.

Possible reduction to lump sum cost of traffic control if contractor elects not to use barrels for all devices on high speed facilities, only for the required taper.

2. Operational (For example, maintenance, materials, equipment, labor, administrative, programming).

Will require coordination with law enforcement to provide officer and equipment. May require additional resources from contractor/maintenance if closure

3. Life cycle cost.

N/A

G. Benefits? (Provide details that can be used to complete a Cost – Benefit Analysis.) (Estimates are acceptable.) (If no costs, what is the benefit of making this change?)

We have experienced severe crashes, and adherence to this Specification should eliminate this issue.

H. Safety Impacts?

Provide better coordination among the various groups responsible for safety on a project (Traffic and Safety, Construction, Maintenance, and the Contractor). We have experienced severe crashes, and adherence to this Specification should eliminate this issue.

I. History? Address issues relating to the current usage of the item and past reviews, approvals, and/or disapprovals.

This is the second time this has been presented to the Standards Committee. It has been prepared at the request of Senior Administration.






Priority Explanation

Enter the appropriate priority in the box on the first page of the document.




- | | |
|------------|---|
| Priority 1 | Upon posting, this impacts all projects in construction and design with a Change Order, Addenda, and immediate change to projects being advertised. |
| Priority 2 | Upon posting, this impacts projects being advertised. |
| Priority 3 | Upon posting, the approved standard takes effect four weeks later for projects being advertised. |

Message Id: 47E28E60.161 : 156 : 5649
Subject: Proposed Modification to Standard Specification 01554, Traffic Control
Created By: JLEONARD@utah.gov
Scheduled Date:
Creation Date: 03/20/08 4:18 PM
From: John Leonard

Recipients

Recipient	Action	Date & Time	Comment
 civilsience.com	Transferred	03/20/08 4:19 PM	
To: Tyler Yorgason (tyorgason)			
 dot.gov	Transferred	03/20/08 4:19 PM	
To: Anthony Sarhan (anthony.sarhan)			
CC: Roland Stanger (Roland.Stanger)			
 gcinc.com	Transferred	03/20/08 4:19 PM	
To: Mont Wilson (mont.wilson)			
 SRCOPO1.SRDOMAIN	Delivered	03/20/08 4:18 PM	
CC: Barry Axelrod (BAXELROD)	Read Replied	03/21/08 9:36 AM 03/21/08 9:43 AM	
CC: Garr Ovard (GOVARD)	Read	03/20/08 4:22 PM	
To: Glenn Schulte (GSCHULTE)	Read Replied	03/24/08 6:45 AM 03/25/08 8:12 AM	
To: Greg Searle (GSEARLE)	Read	03/24/08 8:13 AM	
CC: John Leonard (JLEONARD)	Read	03/20/08 4:18 PM	
To: Justin Sceili (JSCEILI)	Read Forwarded Replied Deleted Emptied	03/20/08 10:50 PM 03/20/08 10:53 PM 03/21/08 3:26 PM 03/21/08 3:26 PM 03/29/08 1:28 AM	
CC: Kris Peterson (KRISPETERSON)	Read Forwarded	03/24/08 2:22 PM 03/24/08 2:26 PM	
To: Larry Montoya (LMONTOYA)	Read	03/20/08 4:20 PM	
CC: Lynn Bernhard (LYNNBERNHARD)	Read	03/21/08 10:10 AM	
To: Michael Cuthbert (MBCUTHBERT)	Read	03/20/08 4:19 PM	
To: Michael Kaczorowski (MKACZOROWSKI)	Read	03/20/08 5:15 PM	
To: Mike Donovan (MDONIVAN)	Read Replied Deleted Emptied	03/24/08 7:37 AM 03/24/08 1:33 PM 03/24/08 1:39 PM 03/24/08 1:39 PM	
CC: Peter Negus (PNEGUS)	Read	03/21/08 7:41 AM	
To: Richard Clarke (RICHARDCLARKE)			
To: Robert Hull (RHULL)	Read	03/20/08 9:36 PM	
CC: Robert Miles (ROBERTMILES)	Read Deleted Emptied	03/21/08 9:41 AM 03/21/08 9:41 AM 03/29/08 1:38 AM	
To: Rukhsana Lindsey (RLINDSEY)	Read	03/31/08 10:58 AM	
CC: Stan Adams (STANADAMS)	Read Replied Deleted Emptied	03/24/08 7:06 AM 03/24/08 7:20 AM 03/24/08 7:20 AM 03/24/08 7:20 AM	
To: Stan Burns (SBURNS)	Read Deleted Emptied	03/21/08 9:37 AM 03/21/08 9:39 AM 03/21/08 2:31 PM	
To: Tim Biel (TBIEL)	Read Deleted Emptied	03/24/08 2:26 PM 03/24/08 3:19 PM 03/24/08 3:22 PM	
To: W. Scott Jones (WSJONES)	Deleted	04/07/08 6:18 PM	
CC: Warren Grames (WGRAMES)	Read Deleted	03/20/08 4:20 PM 04/01/08 1:05 PM	
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To: Carrie Jacobson (CJACOBSON)	Read	03/21/08 12:14 PM	
CC: Cory Pope (CORYPOPE)	Read Deleted	03/20/08 6:03 PM 04/02/08 9:54 PM	
To: Darin Duersch (DDUERSCH)			

To: Evan Sullivan (EVANSULLIVAN)	Read	03/21/08 8:28 AM
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To: Kelly Barrett (KBARRETT)	Read Replied Deleted	03/25/08 7:36 AM 03/25/08 8:17 AM 03/25/08 8:40 AM
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To: Nick Peterson (NPETERSON)	Read	03/20/08 5:10 PM
To: Rex Harris (REXHARRIS)		
To: Scott Nussbaum (SNUSSBAUM)	Read Replied Deleted Emptied	03/21/08 4:57 PM 03/21/08 5:00 PM 03/24/08 7:45 AM 03/27/08 6:24 AM
To: Steven Niebergall (SNIEBERGALL)	Read	03/21/08 2:22 PM
To: Tommy Vigil (TOMMYVIGIL)	Read	03/21/08 9:45 AM
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To: Betty Purdie (BPURDIE)	Read Replied Deleted	03/21/08 10:11 AM 04/01/08 9:28 AM 04/01/08 9:30 AM
To: Darren Rosenstein (DROSENSTEIN)	Read	03/23/08 10:16 PM
To: Deryl Mayhew (DMAYHEW)	Read Deleted	03/20/08 4:24 PM 03/20/08 4:25 PM
To: Dottie Weese (DWEESE)	Read Replied	03/21/08 6:32 AM 03/25/08 2:58 PM
CC: Jason Davis (JASONDAVIS)	Read Deleted Emptied	03/24/08 1:16 PM 03/24/08 1:16 PM 03/24/08 4:07 PM
To: Josh VanJura (JVANJURA)	Read Deleted	03/25/08 9:27 AM 03/25/08 9:27 AM
To: Lisa Wilson (LWILSON)	Read Replied Deleted Emptied	03/20/08 4:19 PM 03/24/08 9:51 AM 03/24/08 9:51 AM 04/01/08 1:06 AM
To: Lonnie Marchant (LMARCHANT)	Read Deleted Emptied	03/20/08 4:39 PM 03/24/08 7:36 AM 04/01/08 1:06 AM
To: Mark Velasquez (MVELASQUEZ)	Read Forwarded Replied	03/24/08 7:30 AM 03/25/08 8:19 AM 04/01/08 8:41 AM
To: Marwan Farah (MFARAH)	Read	03/24/08 7:42 AM
To: Michelle Page (MICHELLEPAGE)	Read Replied	03/21/08 1:44 PM 03/24/08 7:55 AM
To: Randy Park (RPARK)	Read Deleted	03/24/08 8:46 PM 03/24/08 8:46 PM
To: Rob Wight (RWIGHT)	Read	03/20/08 6:14 PM
 SRR3PO1.SRDOMAIN	Delivered	03/20/08 4:18 PM
CC: Dal Hawks (DHAWKS)	Read Deleted Emptied	03/21/08 9:34 AM 03/21/08 9:34 AM 03/23/08 11:00 AM
CC: David Nazare (DNAZARE)	Read Forwarded Deleted Emptied	03/20/08 5:19 PM 03/21/08 7:45 AM 03/21/08 7:45 AM 04/05/08 1:01 AM
To: Doug Bassett (DBASSETT)	Read Replied Deleted Emptied	03/20/08 5:26 PM 03/26/08 5:22 PM 03/26/08 5:22 PM 03/26/08 5:25 PM
To: Jack Lyman (JACKLYMAN)	Read Deleted Emptied	03/20/08 4:20 PM 03/28/08 9:02 AM 04/05/08 1:01 AM
To: Jim Golden (JIMGOLDEN)	Read	03/21/08 2:08 PM
To: Robert Westover (RWESTOVER)	Read Replied Deleted	03/20/08 5:26 PM 03/20/08 5:29 PM 03/20/08 5:30 PM

To: Scott Andrus (SCOTTANDRUS)	Read	03/20/08 5:18 PM
To: Steve Bonner (STEEBONNER)	Read Replied Deleted Emptied	03/20/08 5:04 PM 03/26/08 1:59 PM 03/26/08 1:59 PM 04/03/08 1:01 AM
 SRR4PO1.SRDOMAIN	Delivered	03/20/08 4:19 PM
To: Anne Ogden (ANNEOGDEN)	Read Replied	03/20/08 4:55 PM 03/31/08 4:00 PM
To: Bret Sorenson (BSORENSEN)	Read Replied Emptied	03/25/08 7:47 AM 03/25/08 8:16 AM 03/25/08 8:23 AM
To: Carl Johnson (CARLJ)	Read	03/20/08 4:22 PM
To: Clark Mackay (CLARKMACKAY)	Read Replied	03/24/08 8:34 AM 03/24/08 11:51 AM
To: Dale Stapley (DSTAPLEY)	Read Deleted Emptied	03/24/08 7:00 AM 03/26/08 1:31 PM 03/26/08 1:34 PM
To: Dan Webster (DWEBSTER)	Read Replied Deleted Emptied	03/26/08 4:20 PM 03/26/08 5:11 PM 03/26/08 5:11 PM 03/27/08 7:33 AM
To: Fred Jenkins (FJENKINS)	Read Replied	03/25/08 7:37 AM 03/25/08 7:41 AM
To: George Leighton (GLEIGHTON)	Read Emptied Deleted	03/21/08 8:22 AM 04/02/08 7:58 AM 04/02/08 7:58 AM
To: Hugh Kirkham (HKIRKHAM)		
To: Jim McConnell (JMCCONNELL)	Read	03/21/08 3:09 PM
To: Lief Condie (LCONDIE)		
To: Lyndon Friant (LFRIANT)	Read Deleted	03/24/08 8:02 AM 03/24/08 8:13 AM
To: Marsha Chaston (MARSHA)	Read Deleted Emptied	03/24/08 7:26 AM 03/24/08 7:30 AM 03/26/08 2:23 PM
To: Mike Seng (MSENG)	Downloaded	03/20/08 4:23 PM
To: Nancy Jerome (NJEROME)	Read Deleted Emptied	03/27/08 10:52 AM 03/27/08 11:00 AM 03/28/08 9:40 AM
To: Nathan Merrill (NMERRILL)	Downloaded Read	03/21/08 8:38 AM 03/21/08 8:56 AM
To: Robert Dowell (RDOWELL)	Read Replied	03/20/08 4:57 PM 03/24/08 1:47 PM
To: Russ Tangren (RTANGREN)	Read	03/20/08 5:29 PM
To: Scott Snow (SCOTTSNOW)	Read	03/24/08 7:12 AM
To: Steve Kunzler (SKUNZLER)		
To: Steve Ogden (SOGDEN)	Read	03/20/08 7:48 PM
To: Teri Peterson (TERIPETERSON)	Read Deleted Emptied	03/24/08 2:12 PM 03/24/08 2:12 PM 04/03/08 8:57 AM
To: Troy Torgersen (TTORGERSEN)	Read	03/24/08 7:02 AM
 SRTCPO1.SRDOMAIN	Delivered	03/20/08 4:18 PM
To: Danielle Herrscher (DANIELLEHERRSCHER)	Read Replied	03/20/08 8:50 PM 03/26/08 5:04 PM
To: Dave Kinnecom (DKINNECOM)	Read	03/20/08 4:20 PM
To: Eric Rasband (ERASBAND)	Read Deleted	03/20/08 6:03 PM 03/24/08 7:41 AM
To: Rob Clayton (ROBERTCLAYTON)	Read	03/20/08 4:24 PM
To: Troy Peterson (TLPETERSON)	Read	03/25/08 9:06 AM
 utah.gov	Transferred	03/20/08 4:19 PM
To: Robert Markle (RMARKLE)		

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SRR4PO1.SRDOMAIN	03/20/08 4:19 PM	utah.gov
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Files

File	Size	Date & Time
MESSAGE	1067	03/20/08 4:18 PM
Spec 01554 Submittal 3-08.pdf	28819	03/20/08 4:08 PM

Options

Auto Delete:	No
Concealed Subject:	No
Expiration Date:	None
Notify Recipients:	Yes
Priority:	Standard
Reply requested by	Thursday, April 03, 2008
Security:	Standard
Send Notification	when Opened
Send Notification	when Deleted
To Be Delivered:	Immediate

Standard Drawing/Specification Review Sheet		Review Comments		
STD DWG/Spec Number	01554	Sheet 1	of	7
Date:	April 2008	Facilitator:	John Leonard	

Review Comments Form

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
1	Robert Westover, R-3		No Comment	A	A
			Response:		
2	Barry Axelrod, Central		It looks like the second page of the supplemental is missing from the file you sent out.	A	A
			Response: Specification was reformatted to be on one page for ease of viewing and printing. Footer did not reformat automatically.		
3	Justin Sceile, Statewide Permits		Looks good to me	A	A
			Response:		
4	Scott Nussbaum, R-1 Materials		I take no exception to the change	A	A
			Response: Drawing modified with correct border		
5	Keith Bladen, R-1 Risk Management		The modification looks good to me	A	A
			Response:		
6	Stan Adams, Statewide Construction		I have no comments or issues with this modification	A	A
			Response:		
7	Michelle Page, R-2 Construction		Looks Good	A	A
			Response:		
8	Lisa Wilson, R-2 Traffic		This looks good	A	A
			Response:		
9	Clark Mackay, R-4		The bottom line says page 1 of 2. I did not receive any part of page two. Should it read page 1 of 1 or is 2 really missing. I did not have any comments on the rest of the document	A	A
			Response: You are right--the footer is wrong. I reformatted it so it would be on one page to make it easier to review, but didn't notice the footer hadn't changed with it.		
10	Robert Dowell, R-4 Richfield		I have no comments on the proposed changes.	A	A
			Response:		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet		Review Comments		
STD DWG/Spec Number	01554	Sheet 2	of	7
Date:	April 2008	Facilitator:	John Leonard	

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
11	Fred Jenkins, R-4 Const		Does this apply to all roads, not just the interstate?	B	A
			Response: It is applicable to all roads. However, it is only to be used when adequate gaps are not present. If the gaps in traffic are sufficient, then this process is not required.		
12	Glenn Schulte, Statewide Traffic and Safety		I like the changes. One question--can this be used on two lane highways or just multi lane roadways, or is it intended for just freeways. Some verbiage should be included on which highway types this should be used on.	B	A
			Response: It is applicable to all roads. However, it is only to be used when adequate gaps are not present. If the gaps in traffic are sufficient, then this process is not required.		
13	Dottie Weese, R-2 Risk Management		Looks good to me	A	A
			Response:		
14	Steve Bonner, R-3 Risk Management		No comment, looks good to me	A	A
			Response:		
15	Dan Webster, R-4 Cedar Risk Management		No comments, thanks	A	A
			Response:		
16	Doug Bassett, R-3 Traffic		No comment, looks fine to me	A	A
			Response:		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet		Review Comments		
STD DWG/Spec Number	01554	Sheet 3	of	7
Date:	April 2008	Facilitator:	John Leonard	

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
17	Mike Donovan, Statewide Traffic and Safety		<p>If we are changing this specification we should also change part 2, 2.2 . to read:</p> <p>C. Clothing 1. Flagger Vest and Hard hat: Orange, red-orange, or fluorescent version of these colors: a. For daytime and nighttime activity, flaggers shall wear safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel" and labeled as meeting the ANSI 107-2004 standard performance for Class 2 risk exposure.</p> <p>For nighttime activity, safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel" and labeled as meeting the ANSI 107-2004 standard performance for Class 3 risk exposure should be considered for flagger wear.</p> <p>3.3 1. Use plastic drums or directional barricades as lane closure taper devices for speeds 50 mph and greater.</p> <p>3.3 C. 1. (Construction Clear Zone) should be changed to (Work Clear Zone)</p>	B	D
			Response: Will forward to Robert Miles for consideration as editorial comments. Otherwise, will have to follow modification process		
18	Bret Sorenson, R-4 Design		<p>Here are my comments:</p> <p>1 - Should Section 1.4.A read "as defined by Region Traffic Engineer" instead of "as approved by the Region Traffic Engineer"?</p> <p>2 - Is the "Traffic Slow Down" a reduction from the regularly posted speed or the posted speed during construction?</p> <p>3 - Who decides what construction activities warrant a "Traffic Slow Down"?</p>	<p>B</p> <p>A</p> <p>B</p>	<p>D</p> <p>C</p> <p>C</p>

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet		Review Comments		
STD DWG/Spec Number	01554	Sheet 4	of	7
Date:	April 2008	Facilitator:	John Leonard	

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
			Response: Will evaluate the terminology of defined v approved. The slow down is a significant reduction, possibly to an extremely slow rate of speed. Activities that warrant a slow down are those that can not get an acceptable gap in the existing traffic stream to get the work completed. This would be a discussion between the RE and the contractor, and, if the work warranted it, the slow down spec would be implemented.		
19	Kelly Barrett, Region 1 Maintenance		Peak hours..... I think the Region Traffic Engineer should be involved in this discussion but in nearly all cases the RE has better knowledge, or a better idea of what's going on in the field than does the RE. Maybe it could read approved by the RE with concurrence of The Region Traffic Engineer.	B	C
			Slow down..... A.1.c is very confusing to me. What do you consider the first two lanes? And why does that have to be done using an officer in a marked car? If we choose to use the officer and marked car that's fine, just asking the ?	B	C
			Second, with regards to the slow down. Why are we wanting to get approval two days in advance? We allow delays all the time. A rolling slow down is simply a minor delay. If one is needed I think we should be able to use one with the approval of the RE regardless of when they ask.....	B	C
			Response: The RE may have better knowledge of what is going on within the project, but the RTE has a better overall general knowledge of what is going on from a systemic point of view. The RTE is better equipped to make the decision based on Region-wide issues. The intent of A.1.c is to require an officer for these planned events. Added language to clarify the one or two lanes. These slow downs are planned events, and can provide the required notification. This will allow coordination with other activities and entities to make sure the slow down can be accomplished in the safest manner.		

Action Code	A	B	C	D
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Standard Drawing/Specification Review Sheet		Review Comments		
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Date:	April 2008	Facilitator:	John Leonard	

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
20	Danielle Herrscher, R-2 Traffic		Regarding the Traffic Control spec, I am wondering if you need to define the first two lanes. I am assuming most contractors understand this is lane #1 and 2 from the center median but have had a couple of people ask me to define the lane numbers for them.	B	C
			Response: The reference of the 'first two lanes' is to the total number of lanes slowed, not their location within the traveled way. The officer can slow up to two lanes, and then either additional officers or contractor personnel can then assist to slow any additional lanes. Added language to clarify the two lanes.		
21	Anne Ogden, R-4 Traffic		I have a few comments on the proposed modification to Section 01554...		
			--#2 under letter "D" doesn't seem to be complete. The sentence or paragraph needs to be finished.	B	C
			--Do you need to explain in letter "F" why it is the "second time" this has been presented? What happened the first time? Have revisions been made?	B	C
			--Is this for any highway? Or only for interstate highways? Should that be clarified?	B	C
			--Reword 3.7.A.1.c to say "Use, in any combination, either <i>contractor-supplied</i> vehicles.....or additional officers in marked..."?	A	A
			--Is "slow-moving vehicles" in 3.7.A.2 referring to construction vehicles for which the slow down is occurring? Should this be clarified and/or differentiated from "slow-moving vehicles" that would be in the normal traffic flow?	B	A
			--Fix the font size inconsistencies in 3.7.A.3.	A	A
			--Is there supposed to be a "Page 2 of 2" attached also?	B	C

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	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet		Review Comments		
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Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
			<p>Response: Reviewed section—left as is</p> <p>First submission was originally placed in limitation of operations. Was directed by the committee to place it in 1554 instead. Wording intent has not changed, but some of the stakeholders change with the different specification. Was placed on agenda to allow those stakeholders to comment.</p> <p>It is applicable to all roads. However, it is only to be used when adequate gaps are not present. If the gaps in traffic are sufficient, then this process is not required.</p> <p>Punctuation corrected.</p> <p>Deleted the reference to slow moving vehicles.</p> <p>Font inconsistencies were in review copy only—no changes.</p> <p>You are right--the footer is wrong. I reformatted it so it would be on one page to make it easier to review, but didn't notice the footer hadn't changed with it.</p>		
22	Mark Velasquez R-4 ROW		No Comments	A	A
			Response:		
23	Betty Purdie, R-2 Operations		<p>I have a couple of comments:</p> <p>-Don't define the peak hours, since these vary from route to route (if defined then we will need a special on every project the they differ) 3.7 A.2. states that they can not do this during peak hours as defined by TE, so we should be covered by that.</p> <p>-The traffic slow down is requiring overhead flashing lights (many officers no longer have overhead lights, so do we just want to call for flashing lights instead? Same with the over head amber beacon - not all of our vehicles have that type of lighting.</p>	<p>A</p> <p>A</p>	<p>C</p> <p>C</p>

Action Code	A	B	C	D
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Standard Drawing/Specification Review Sheet		Review Comments		
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Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
			Response: Typically, this will be used in urban areas where there are not available gaps in the traffic stream. The peak hour restrictions are typical for these areas. The specification may be used in rural areas where gaps are not available—if so, and the Region desires it, the peak hours can be modified for that particular project. However, we believe that the generic peak hours will be effective for most applications of this specification. Overhead lights are required for visibility of following vehicles. Overhead lights are visible from behind over many car lengths---flashing lights on the corners of the vehicle are obscured by the first trailing vehicle, and are not visible as advance warning for vehicles approaching from the rear. The advance notification provides opportunity to ensure the right vehicle selection.		
24	Tyler Yorgason, ACEC		I have no specific comments and have received no comments to pass on from other ACEC reviewers regarding this proposed modification to Standard Specification 01554. Thanks again for the chance to review this change.	A	A
			Response:		
51	Mont Wilson, AGC		Called when no written response. Mont returned call and indicated that he saw no issues that would affect the AGC members at this time.	A	A
			Response:		
52	Roland Stanger, FHWA		Replace 'public safety' with 'law enforcement'	A	A
			Replace '2 days' with 48 hours	A	A
			Add 'one or' to 'two lanes' to show the minimum number of lanes a law enforcement officer can control	A	A
			Use 'lights' instead of 'beacons'	A	A
			Response: Editorial comments accepted		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

**Supplemental Specification
2008 Standard Specification Book**

SECTION 01554M

TRAFFIC CONTROL

Delete Article 1.3, and replace with the following:

- A. AASHTO Roadside Design Guide, Current Edition
- B. American National Standards Institute (ANSI)
- C. ATSSA: American Traffic Safety Services Association
Quality Standards for Work Zone Traffic Control Devices
- D. International Safety Equipment Association (ISEA)
- E. Manual on Uniform Traffic Control Devices (MUTCD), Current Edition
- F. NCHRP- Report 350 Recommended Procedures for the Safety
Performance Evaluation of Highway Features
- G. UDOT Flagger Training Handbook
- H. UDOT Guidelines for Crash Cushions

Delete Article 1.4, and replace with the following:

1.4 DEFINITIONS

- A. Peak Hours: 6:30 a.m. to 9:00 a.m., 3:30 p.m. to 7:00 p.m., Monday-Friday, or as approved by the Region Traffic Engineer.
- B. Traffic Slow Down: An isolated planned event where traffic on a highway is reduced in speed to provide a gap for work to proceed.
 - 1. Examples include the crossing of the highway with heavy equipment or the adjustment of traffic control devices.

Delete Article 1.10, Paragraph B, and replace with the following:

- B. Meet all requirements of this Section, article 1.9, Traffic Control Maintainer when traffic control devices are required to be in place overnight or on weekends.

Delete Article 2.2, Paragraph C 1a, and replace with the following:

- a. Wear safety apparel for daytime and nighttime activity meeting the requirements of ISEA "American National Standard for High-Visibility Apparel" and labeled as meeting the ANSI 107-2004 standard performance for Class 2 risk exposure.
 - 1) Considered for flagger wear for nighttime activity, safety apparel meeting the requirements of ISEA "American National Standard for High-Visibility Apparel" and labeled as meeting the ANSI 107-2004 standard performance for Class 3 risk exposure.

Delete Article 3.3, Paragraph A1, and replace with the following:

- 1. Use plastic drums or directional barricades as lane closure taper devices for speeds 50 mph and greater.

Delete Article 3.3, Paragraph C1, and replace with the following:

- 1. Remove traffic control devices from the roadway a distance twice that of the Work Clear Zone if they will be used within 24 hours of the daily work stoppage and are not required for immediate traffic control. Refer to the TC Series Standard Drawings.
 - a. Obtain written permission from property owner prior to storing traffic control devices on private property.

Add the following to Part 3:

3.7 LIMITATION OF OPERATIONS

- A. Traffic Slow Down
 - 1. Notify and obtain approval from the Department and law enforcement a minimum of 48 hours prior to slow down.
 - 2. Use a Highway Patrol Trooper or other law enforcement officer in a marked vehicle with overhead flashing lights to conduct the slowdown.
 - 3. Use the officer in the marked vehicle to slow down one or two lanes. Use, in any combination either, contractor-supplied vehicles equipped with overhead amber flashing lights or additional officers in marked vehicles at the rate of one vehicle per lane thereafter for all lanes of the highway to affect the traffic slow down.
 - 4. Additional vehicles as described in this Article may be used in the traffic slow down to supplement the law enforcement vehicle when required by the officer.
 - 5. No traffic slow downs will be allowed during peak hours, holiday periods, or events defined by the Region Traffic Engineer.
 - 6. The length of duration of any traffic slowdown not to exceed five minutes or as approved by the Region Traffic Engineer.

Standards Committee Submittal Sheet

Name of preparer: Robert Miles

Title/Position of preparer: Preconstruction and Standards Engineer

Specification/Drawing/Item Title: Relocate DD 1 thru DD 16 from Std Drawings to MOIs

Specification/Drawing Number: Relocate DD 1 thru DD 16 from Std Drawings to MOIs

Enter appropriate priority level:

(See last page for explanation) 3

Sheet not required on editorial or minor changes to standards. Check with Standards Section.

NOTES:

1. All Submittal Sheets must be completed and sent to the Standards Section by the Standards Committee suspense date as shown on the Web.
(<http://www.udot.utah.gov/go/standardscommittee>)
2. The Preparer of the Submittal Sheet or the Standards Committee member (or authorized substitute) responsible for the submittal must be present at the Standards Committee meeting and capable of discussing and answering all questions related to the submittal. The item will be postponed to a later meeting if one of these people is not present.
3. Notify the Standards Section immediately of any changes that impact the presentation to include absence of sponsor or delay in presentation.

Complete the following: (Use additional pages as needed.)

- A. Why? Detail the reason for changing the Standard (Specification or Drawing), what has initiated a new Standard, or what has caused a new or changed item of interest.

For many years we have maintained a set of DD (design) drawings as part of our standard drawings. By and large there is very little in these drawings that can be applied in the “field” by a contractor, inspector or maintenance crew. Nearly all of the information included in the drawings is meant to be applied by personnel involved in preconstruction activities. This information should be more closely tied the information contained in our Roadway Design Manual of Instruction and Our Structural Design Manual of Instruction. Placing the information in these manuals will lead us to better utilize the information. We have put considerable effort into creating and updating our manuals of instruction as a method of communicating UDOT’s approach to the AASHTO manuals. Relocating the information contained in the DD series of standard drawings into the manuals of instruction will encourage individuals to utilize these resources, consolidate the presentation of information and improve the correlation of information contained in the manuals of instruction and that communicated in the current standard drawings.

At least on the of the DD drawings, DD 2, should be maintained as part of the traditional standard drawings book. This drawing could be relocated to the GW section.

Providing the DD drawings within the Roadway Design Manual of Instruction (and Structure's Design Manual of Instruction when applicable) would provide our access management personnel more information to use with engineering firms working for developers.

Control or input over the content of the relocated drawings should remain with the standards committee. The drawings would remain standards and be maintained in the same manner that they have been in the past.

B. Measurement, Payment, Acceptance, and Documentation:

1. How is Measurement and Payment handled? Existing (from the measurement and payment document), modified, or new measurement and payment to be included with all Standard Specifications or Supplemental Specifications.

No effect

2. How is Acceptance and Documentation handled? Existing (from the acceptance and documentation document), modified, or new acceptance and documentation to be included with all Standard Specifications or Supplemental Specifications. Include Contractor Submittals, Inspection Elements, and Documentation.

No effect

C. Stakeholder Notification for AGC and ACEC:

By email provide the AGC and ACEC Standards Committee member a copy of all pertinent information relating to the specification or drawing. Detail all responses below. Indicate if no comments were received.

Note: There is a two-week response time set for this item.

Refer to the Standards Committee Web site, Members page at <http://www.udot.utah.gov/main/f?p=100:pg:::::1:T,V:659> for the respective e-mail addresses.

AGC Comments: (Use as much space as necessary.)

None received

ACEC Comments: (Use as much space as necessary.)

Please see attached comments

- D. Stakeholders? From the list provided, document the stakeholders contacted, detailing: the company, name of contact, how contacted (by phone, email, hard copy, or in person), concerns, and comments of the change. Stakeholders:

Note: There is a two-week response time set for this item. Allow Stakeholders two weeks to process and respond to coordination requests. All areas should try to complete review and comment as soon as possible but within two weeks.

In-house (for example, preconstruction, materials, construction, safety, design, maintenance) (Include all applicable in-house areas even if not listed above.)

Construction Engineers

Please see attached comments

Contractors (Any additional contacts beyond “C” above.)

Suppliers

Consultants (as required) (Any additional contacts beyond “C” above.)

Please see attached comments

FHWA (To be accomplished as part of the two-week process before submitting to the Standards for inclusion on the Standards Committee agenda.) (This is in addition to the requirements of UDOT Policy 08A5-1, procedure 08A5-1.3.)

Please see attached comments

Others (as appropriate)

- E. Other impacted areas, systems, or personnel. (Consider all impacts and possible changes to these areas during the preparation process. Coordinate with all appropriate areas for the respective item. List all impacts and action taken.)

1. Minimum Sampling and Testing Requirements

No effect

2. Business Systems (Electronic Bid System, Project Development Business System, Electronic Program Management, Computer-Aided Drafting and Design, etc.)

No effect

3. Implementation Plan (Provide detailed instructions on how the subject item will be implemented to include notification of all interested parties and training requirements.)
Training and coordination will need to be implemented in order to ensure that all issues described in the drawings are relocated to the appropriate guides, manuals or other sections of the standard drawings. Additional training will also need to be implemented to help enforce the idea that design guidance should be based in our manuals of instruction.
- F. Costs? (Estimates are acceptable.)
1. Additional costs to average bid item price.
No effect
 2. Operational (For example, maintenance, materials, equipment, labor, administrative, programming).
No effect
 3. Life cycle cost.
No effect
- G. Benefits? (Provide details that can be used to complete a Cost – Benefit Analysis.) (Estimates are acceptable.) (If no costs, what is the benefit of making this change?)
Additional benefits will be gained by providing fewer separate sources of design guidance for individuals, consultants and new employees, which are working on design issues for the department of transportation. Use of standardized guidance would bear additional benefits in areas such as access management where we are working with consulting firm and individuals that do not have the experience of working with the Department on a regular basis. If these “standards” are located in our manuals of instruction we would be more likely to maintain the critical areas of information.
- H. Safety Impacts?
None
- I. History? Address issues relating to the current usage of the item and past reviews, approvals, and/or disapprovals.

Priority Explanation

Enter the appropriate priority in the box on the first page of the document.

- | | |
|------------|---|
| Priority 1 | Upon posting, this impacts all projects in construction and design with a Change Order, Addenda, and immediate change to projects being advertised. |
| Priority 2 | Upon posting, this impacts projects being advertised. |
| Priority 3 | Upon posting, the approved standard takes effect four weeks later for projects being advertised. |

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Date:		Facilitator:		

Review Comments Form

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
1	Fred Doehring (email)		As we have talked about in the past, I fully support this move. Response: None Required		
2	Robert Westover (email)		no comment Response: None Required		
3	Scott Andrus (email)		I have no problem with moving them to a MOI so long as everyone within the Dept or hired by the Dept is informed and has easy access. Response: Communication Plan needs to be fully developed.		
4	Brent Schvaneveldt (email)		Sounds good to me. Response: None Required		
5	Richard Miller (email)		I am in agreement with your proposal. Response: None Required		
6	Todd Richins (email)		I agree with what you are proposing. ... Thank you for keeping us in the loop of things, talk with you later. Response: None Required		
7	Daniel Young (email)		Looks like a good change...I haven't any comments. Response: None Required		
8	Robert Stewart (email)		The only issue I see will be in the precedence of documents in design-build. The Standard Drawings usually fall #2 or #3 in a priority list, and the Roadway MOI is below the AASHTO Green Book (#7 or #8). Response: Will work with Robert Stewart to coordinate this issue depending on the out come of the proposal. I believe this would require moving the roadway design manual of instruction up in the order of precedence.		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		Sheet 2	of	8
Date:		Facilitator:		

9	Anne Ogden (email)	<p>...it's probably not a good argument to say that you're moving the DD drawings to a resource that people don't really use. I've been out of design for a couple of years, but I think the design MOI is still fairly new to a lot of designers and they may not use it as much as they should. Even so, I think it's probably better to only use the justification of putting designers' resources in fewer locations and not go on to say that it would encourage them to use the MOI more because they don't use it enough now.</p> <p>Response: Agreed, will update application to better state pros and cons.</p>		
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10	Anne Ogden (email)	<p>Also, it's never actually explicitly stated that the drawings will go in the "Design MOI"; it just says "our manuals of instruction". Could that be clarified?</p> <p>Response: This will be clarified in the application process. Under this proposal most of the DD drawings would be relocated to the Roadway Design Manual of Instruction. Some would be better served in the Structures Design Manual of Instruction.</p>		
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11	Anne Ogden (email)	<p>I do have concerns about how easily accessible the information will be to those of us who aren't currently designers. I have used the turn lane standard drawings several times in the past year when looking at striping and turn lane dimensions. How would they be available to those of us who don't have the design MOI (or wherever they're included)?</p> <p>Response: The Roadway Design Manual of Instruction is available on line in the Consultant and Design Resources are.</p>		
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12	Doug Bassett (email)	<p>I don't necessarily have a problem with moving the drawings to the MOI. However, I don't think it's true that the DD 14 and DD 15 series of drawings are mostly used by preconstruction personnel. My experience is that we require developer's engineers to use the drawings for new roads and accesses in their design far more often than designers do on projects that are under the Preconstruction Engineer's responsibility, or at least it seems so to me.</p> <p>Response: I agree that these particular drawings are widely used by engineer's working for developers requesting access to our system. Many of these design firms do not normally perform work for the Department and are more qualified to perform site development work. Locating these drawings in our manual of instruction would provide</p>		
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Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		Sheet 3	of	8
Date:		Facilitator:		

13	Doug Bassett (email)	I believe it is important to have the drawings still in a drawing form for the developer's engineers to use so they can design easier and so we maintain some kind of consistency in the field. I realize not every new road or access connection can adhere to the standard, and so maybe it's a good thing to not keep them as a standard so there is some flexibility in the design so it fits field conditions. And there are a few things in the DD 14 and 15 series I don't totally agree with; maybe I could allow the designers to not include these things when they are laying out a design.		
		Response: Please see response to comment 12		

14	Doug Bassett (email)	It's probably none of my business, but several people put a lot of effort into making the DD 14 and 15 series what they are currently. Are they okay with moving them to the MOI which in effect takes away some of their effort?		
		Response: Additional coordination is now occurring.		

15	Robert Hull (email)	Perhaps I'm missing something here, but I'm very concerned where this is going. We are moving away from standards to guidance? What mechanism ensures the roadway is designed to some sort of minimum requirement? Policy? Law? Please help me understand.		
		Response: Additional coordination is now occurring		

16	Darin Duersch (verbal)	Why are we doing this, what is the benefit to UDOT? Is this an exercise in shuffling papers?		
		Response: Additional coordination is now occurring		

17	Betty Purdie (email)	I disagree with moving these drawings out of the standards. Although they may not be used often, they are used in construction and maintenance. If they are relocated to a MOI, they then will not be a handy resource when needed. If they are going to be kept up to date, and still go through standards, then they should be a resource for everyone. So I would suggest adding them to the MOI and keeping them in the standard drawings.		
		Response: Additional coordination is now occurring		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		Sheet 4	of	8
Date:		Facilitator:		

18	Dave Schwartz (email)		You are right that most of the DD are for the designer. DD 16 is the only one that maybe the contractor needs to see and that could be moved to structures or GW. If you do this you may want to think about the ST sheets. ST 1,2 and 8 are the only ones that may be useful for the contractor and those could be put in the SN sheets (and to tell the truth I don't know why they are not). The striping should really be laid out well in the plan sheets.		
			Response: Additional coordination is now occurring		

19	Dave Schwartz (email)		My concern for moving them from the STD is not with the designers but with the construction and Maintenance people. If there is a change order that adds one of these things or changes one, the STD is the place that these people will go and look. Or if it is an Orange book or purple book project these tend not to have a lot of detail as far as the striping and grading on the shoulders and these are found in these areas. That may be where it is useful to the contractor. I don't think this is a deal killer either. You may be able to set up training with these groups and make them aware of the changes so that they can find them when necessary. I may also be wrong and construction and maintenance may never use them. I would check with some like Barney Beckmellon or Steve Nelson from construction and maybe Kevin Ogden or Tyson Vorwaller from maintenance to see if they ever use them.		
			Response: Additional coordination is now occurring		

20	Dave Schwartz (email)		Another area that you would want to be sure to catch are projects such as design build where RFP's go out and those people who put the RFPs together. You would have to be sure to include the MOI as one of the sources that must be followed. This may/might be happening already.		
			Response: Additional coordination is now occurring		

21	Dave Schwartz (email)		My other concern would be giving the standards committee control over your MOI. Today it may just be the drawings but ten years down the road it may be interrupted by others in your job or on the committee that it is for the whole MOI. Just seems like another layer of bureaucracy to get around to get changes made		
			Response: Additional coordination is now occurring		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		Sheet 5	of	8
Date:		Facilitator:		

22	Dave Schwartz (email)	Another thought that I had is if these drawings get moved to the MOI would others get moved to R/W MOI and to traffic and safety's MOI and to the Drainage MOI etc. which are all separate documents which would require the designer and others to check all of these sources.		
		Response: Additional coordination is now occurring		

23	Dave Schwartz (email)	One last thought. If these drawings were to be moved to the MOI you would at least know who owns the drawings and if they needed to be changed that is a benefit.		
		Response: Additional coordination is now occurring		

24	Bret Sorenson (email)	Have field people (contractors, inspectors, and maintenance personnel) confirmed that they don't use these drawings or is it an assumption that they don't? I agree that several of these drawings are not used by "field" people, but would argue that some of the drawings are used by individuals not in preconstruction (DD-2, DD-7, DD-14 to DD-16). I assume our permits people refer people (developers) to our standard drawings on a regular basis. If these drawings are moved to the MOI then obviously they (Operation/Permits) will need to become familiar with that document as well - if they are not already.		
		Response: Additional coordination is now occurring		

25	Bret Sorenson (email)	One might also ask, if the Standards Group is still going to maintain these drawings is it really worth moving them to the MOI?		
		Response: Additional coordination is now occurring		

26	Doug Graham (email Horrocks)	The std dwgs appear to go through a very routine and formal update and review process, and making changes and updates to these details available to the users is an established procedure. For example, if they wanted to revise a drawing to meet updated standards, they get the drawing revised and notification is sent. If the same detail was printed in the MOI, I am concerned at how an update would be made and distributed.		
		Response: Additional coordination is now occurring		

27	Doug Graham (email Horrocks)	Many of the DD drawings list criteria straight form AASHTO. This may be a way to eliminate some of the drawings by referencing AASHTO.		
		Response: Additional coordination is now occurring		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		Sheet 6	of	8
Date:		Facilitator:		

28	Doug Graham (email Horrocks)		Overall, I think it would be a good move. They just need to make sure that the information directed at the contractor is replaced elsewhere or put in the plans.		
			Response: Additional coordination is now occurring		

29	Tyler Yorgason (email)		Probably the most common concern is the possible loss of information directed to the contractor such as benching details (DD-2), lane layout and striping information (DD 14 and DD 15 drawings), and bridge embankment details (DD 16). While most of the information is design oriented and typically shown on design plans, some of this information can be and has been used directly by contractors. It may limit UDOT's flexibility and force more detail into each set of plans. UDOT will probably want carefully to consider input from AGC to identify data/information they would like retained in the Std Dwgs.		
			Response: Additional coordination is now occurring		

30	Tyler Yorgason (email)		If the DD sheets are moved, it would seem like other Std Dwgs or parts of them could be considered as well. Is part of UDOT's strategy to consider other Std Dwg series? Most drawings are focused on information directed at the contractor, but some drawings, or parts of drawings look somewhat design oriented (Clear Zone/Shy Line determination in BA sheets, Accel/Decel Calcs on ST sheets, SW 2, for example). It may be good to evaluate if there are some sheets, or, more likely, if there is design information on some sheets not helpful or intended for the contractor.		
			Response: Additional coordination is now occurring		

31	Tyler Yorgason (email)		Overall, the addition of more graphical information into the MOIs is probably good. As well, it is a helpful trend to identify practical differences between UDOT Standards and the AASHTO Green Book in the UDOT MOIs. We encourage UDOT to look to incorporate additional UDOT specific standardized design info (structures such as box culverts, headwalls and sign structures, for example, or maybe RR crossing details) into appropriate published MOIs.		
			Response: Additional coordination is now occurring		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		Sheet 7	of	8
Date:		Facilitator:		

32	Tyler Yorgason (email)	One of the potential drawbacks to moving the DD drawing information from the Std Dwgs is that information may end up being duplicated in different locations, making maintenance of the information difficult. This led to a related concern that design information in the MOIs be as thoroughly reviewed and evaluated as the information found in the standard drawings and specifications.		
		Response: Additional coordination is now occurring		

33	Tyler Yorgason (email)	Overall, the consultants I have spoken with so far agree that the design information contained in the DD drawings would be most appropriate in the corresponding MOI (Roadway, Drainage, Structures, etc.). They have noted that UDOT should be careful to not lose or needlessly duplicate information moved from DD drawings to the MOIs. They also agree that if the design information is moved from the DD drawings, it should continue to be considered and approved in a way similar to the Standard Drawings or Specifications.		
		Response: Additional coordination is now occurring		

34	Tyler Yorgason (email)	Finally, Robert, one of the consultants wondered if it may be appropriate to use standards update e-mail list(s) to broaden the scope of awareness of these (potential) changes. Thanks again for the chance to look at these issues. I will continue to be in touch as I receive additional information,		
		Response: Additional coordination is now occurring		

35	Matt Wildauer (email Parsons)	I am fine with this change. I don't have a significant issue with it being located in either place - standard drawings or MOI - but do agree with the rationale that it is better placed in the appropriate MOI.		
		Response: Additional coordination is now occurring		

36	Matt Wildauer (email Parsons)	I do have a concern about control of the information in the sheets if they are located in the MOI. As stated in the attached document, there needs to be a process for changes, additions, etc. that would be similar to what is currently the process with the standards. UDOT traditionally has not updated their MOI's regularly, although this has been getting better recently. Perhaps this could be an impetus for keeping these up to date along with the drawings. That could make a change for the better.		
		Response: Additional coordination is now occurring		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet			Review Comments		
Std Dwg/Spec Number			Sheet 8	of	8
Date:			Facilitator:		

38	Anthony Sarhan (email)		We would like some clarification on what this action is intended to accomplish. It appears from the submittal that this is an attempt to increase use and familiarization of the Design Manual of Instruction (DMOI) by placing the DD drawings in the manual thereby forcing designers to use this resource. If this is correct, FHWA would like to know what other actions have been taken up to this point to increase use of the DMOI and the effectiveness of these actions.		
			Response: Additional coordination is now occurring		

39	Anthony Sarhan (email)		Is it the intent of this proposal to remove the DD drawings completely from the Standard Drawings? If so, how do you intend to provide access to other resources (i.e. non-designers) to these drawings if they are needed? FHWA has received anecdotal evidence that non-designers do in fact use the DD drawings from time to time.		
			Response: Additional coordination is now occurring		

40	Anthony Sarhan (email)		How does UDOT intend to maintain the integrity of "standards" in a guidance document? Will designers understand that these are in fact "standards" even though they are not located in the Standard Drawings?		
			Response: Additional coordination is now occurring		

41	Anthony Sarhan (email)		How will the Design Exception, Deviation and Wavier process work if the DD drawings are only in the DMOI?		
			Response: Additional coordination is now occurring		

42	Anthony Sarhan (email)		Item E.3 discusses that training will have to "...be implemented help enforce the idea that design guidance should be based in our manuals of instruction". Would this effort not have the same desired outcome of increasing use and awareness of the DMOI among designers?		
			Response: Additional coordination is now occurring		

43	Anthony Sarhan (email)		Are there any plans to move any of the other standard drawings in the future?		
			Response: Not at this time this proposal is limited to drawings in the DD series only.		

44					
			Response:		

45					
			Response:		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standards Committee Submittal Sheet

Name of preparer: Patrick Cowley

Title/Position of preparer: Preconstruction Resource Engineer

Specification/Drawing/Item Title: Grade Separated Arterial

Specification/Drawing Number: DD 11A

Enter appropriate priority level:

(See last page for explanation) 3

Sheet not required on editorial or minor changes to standards. Check with Standards Section.

NOTES:

1. All Submittal Sheets must be completed and sent to the Standards Section by the Standards Committee suspense date as shown on the Web.
(<http://www.udot.utah.gov/go/standardscommittee>)
2. The Preparer of the Submittal Sheet or the Standards Committee member (or authorized substitute) responsible for the submittal must be present at the Standards Committee meeting and capable of discussing and answering all questions related to the submittal. The item will be postponed to a later meeting if one of these people is not present.
3. Notify the Standards Section immediately of any changes that impact the presentation to include absence of sponsor or delay in presentation.

Complete the following: (Use additional pages as needed.)

- A. Why? Detail the reason for changing the Standard (Specification or Drawing), what has initiated a new Standard, or what has caused a new or changed item of interest.

A Grade Separated Arterial standard is needed to show differences in standards from the existing Freeway and Rural Highway drawings. It is to be a less intrusive solution for facilities that operate similar to Freeways, but are more constrained in space.

- B. Measurement, Payment, Acceptance, and Documentation:

1. How is Measurement and Payment handled? Existing (from the measurement and payment document), modified, or new measurement and payment to be included with all Standard Specifications or Supplemental Specifications.

Existing

2. How is Acceptance and Documentation handled? Existing (from the acceptance and documentation document), modified, or new acceptance and documentation to be included with all Standard Specifications or Supplemental Specifications. Include Contractor Submittals, Inspection Elements, and Documentation.

Existing

C. Stakeholder Notification for AGC and ACEC:

By email provide the AGC and ACEC Standards Committee member a copy of all pertinent information relating to the specification or drawing. Detail all responses below. Indicate if no comments were received.

Note: There is a two-week response time set for this item.

Refer to the Standards Committee Web site, Members page at <http://www.udot.utah.gov/main/f?p=100:pg::::1:T,V:659> for the respective e-mail addresses.

AGC Comments: (Use as much space as necessary.)

No input based on phone follow up with Mont.

ACEC Comments: (Use as much space as necessary.)

See attached.

D. Stakeholders? From the list provided, document the stakeholders contacted, detailing: the company, name of contact, how contacted (by phone, email, hard copy, or in person), concerns, and comments of the change. Stakeholders:

Note: There is a two-week response time set for this item. Allow Stakeholders two weeks to process and respond to coordination requests. All areas should try to complete review and comment as soon as possible but within two weeks.

In-house (for example, preconstruction, materials, construction, safety, design, maintenance) (Include all applicable in-house areas even if not listed above.)

State Preconstruction Engineer
Region Design Squad Leaders
Central Maintenance
District Engineers
Traffic and Safety Engineer
Region Traffic Engineers
Central Construction

Construction Engineers

Contractors (Any additional contacts beyond “C” above.)

Suppliers

Consultants (as required) (Any additional contacts beyond “C” above.)

FHWA (To be accomplished as part of the two-week process before submitting to the Standards for inclusion on the Standards Committee agenda.) (This is in addition to the requirements of UDOT Policy 08A5-1, procedure 08A5-1.3.)

Anthony Sarhan

Others (as appropriate)

- E. Other impacted areas, systems, or personnel. (Consider all impacts and possible changes to these areas during the preparation process. Coordinate with all appropriate areas for the respective item. List all impacts and action taken.)

1. Minimum Sampling and Testing Requirements

No Change

2. Business Systems (Electronic Bid System, Project Development Business System, Electronic Program Management, Computer-Aided Drafting and Design, etc.)

No Change

3. Implementation Plan (Provide detailed instructions on how the subject item will be implemented to include notification of all interested parties and training requirements.)

No additional training will be necessary.

F. Costs? (Estimates are acceptable.)

1. Additional costs to average bid item price.

None

2. Operational (For example, maintenance, materials, equipment, labor, administrative, programming).

None

3. Life cycle cost.

None

G. Benefits? (Provide details that can be used to complete a Cost – Benefit Analysis.)
(Estimates are acceptable.) (If no costs, what is the benefit of making this change?)

It allows for a less intrusive solution for facilities that operate similar to Freeways,
but are more constrained in space.

H. Safety Impacts?

I. History? Address issues relating to the current usage of the item and past reviews,
approvals, and/or disapprovals.

Priority Explanation

Enter the appropriate priority in the box on the first page of the document.

Priority 1 Upon posting, this impacts all projects in construction and design with a Change Order, Addenda, and immediate change to projects being advertised.

Priority 2 Upon posting, this impacts projects being advertised.

Priority 3 Upon posting, the approved standard takes effect **four weeks** later for projects being advertised.

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number	DD 11A	Sheet 1	of	7
Date:	April 2008	Facilitator:	Patrick Cowley	

Review Comments Form

Item No.	Reviewer	Sheet/Section No.	Comment	Review Mtg. Action	Final Action.
1	Robert Westover, R3 Operations Engineer	DD 11A	No comments.		
			Response:		
2	Clark Mackay, R4 Construction Engineer	DD 11A	My comments deal with notes 14 and 15. "Algebraic differential' should be 'algebraic difference'. See AASHTO sheet 307 top of page. In note 14 you show a maximum of 6% AASHTO allows the algebraic difference to not exceed 8%. Do we want to match AASHTO or be tighter than AASHTO? Again see AASHTO sheet 307. The 'algebraic difference' correction should also apply to Std Dwg DD 4 notes 16 and 17 and Std Dwg DD 11 notes 15 and 16.		
			Response: The phrase "Algebraic Differential" has been changed to reflect the AASHTO terminology of "Algebraic Difference." The change has also been noted for DD 11 and DD 4. The intent of the 6% maximum Algebraic Difference is to match the maximum superelevation rate.		
3	Clark Mackay, R4 Construction Engineer	DD 11A	I do not understand how a grade separated arterial can have a median with 14' minimum width. Note 9 says you have to provide a median ditch at least 1' below the bottom of the pavement thickness. The detail implies a slope of 6:1 which means you must have 12' minimum between pavements on each side. With 4' shoulders on each side this means a minimum of 20' and does not include any distance for the 6:1 slope caused by the pavement thickness. If the pavement thickness was 1' this would add an additional 12' of width making the minimum distance at least 32'. I also do not understand the reference to 14' and note 16. Note 16 says you need positive separation(barrier) if the distance is less than 50'.		
			Response : A depressed median is not a requirement for a grade separated roadway. The labels "Divided Median" and "Undivided Median" have been removed to avoid confusion. Note 9 says to use a flat median where you are not able to achieve a 1 foot depth below the pavement thickness. This would lead to a cross section similar to the "Undivided Median" drawing. Note 16 refers to an additional condition if the median is less than 50'. It was placed next to the 14' minimum width to draw the designer's attention to this additional condition.		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number	DD 11A	Sheet 2	of	7
Date:	April 2008	Facilitator:	Patrick Cowley	

4	Clark Mackay, R4 Construction Engineer	DD 11A	As I look at Std Dwg DD 11, I am wondering if you are planning on removing the details that are going to be on sheet DD 11A? Should there be a new sheet DD 11?		
			Response: The two standards are similar in many ways, and we've considered merging the two drawings. However, there is enough dissimilarity to keep them separate for the time being. Many of the notes, much like with DD 4, will be the same for both drawings to prevent confusion.		

5	David Schwartz, R2 Resident Engineer	DD 11A	This MIN needs rethought. If it is 14' need to go to the Undivided Median. For Maintenance purposes a min of 28' from Shld to shld should be paved and over 28' then a depressed median could be put in. Anything less than 28' would be hard to maintain especially if a positive barrier is in place.		
			Response: The labels "Divided Median" and "Undivided Median" have been removed to avoid confusion. The drawing is intended to give a typical view of a roadway with a depressed median, and an overall minimum width for this type of roadway. It is the responsibility of the designer to calculate the needed width to adequately provide a depressed median given the roadway conditions (pavement thickness, etc.)		

6	David Schwartz, R2 Resident Engineer	DD 11A	On the outer edges we seem to be very concerned about being below GB which is good but that concern seems to go away in the middle where water will set longer and it will definitely get into the pavement section. There should be a note about drainage and the kind of material in to be used in the middle like if a concrete ditch is required. The other issue is the 6:1 or flatter. By this note it could be straight across and that is not acceptable. If the min distance of 14' from shld to shld is used at a 6:1 the elevation drops only .5 ft which is in the UTBC layer or still the asphalt layer. If the min slope is 2% (which this should never be less than) the depth of the ditch would only be .72 inches. To summarize or make more understandable, there should be a concrete ditch required and a min. slope defined of no less than 2% for medians less than 50 in width or pave the median		
			Response: Note 4 gives direction for the slope to extend 1 foot below the pavement thickness or provide other measures to drain the pavement thickness layers. As shown on the drawing, this includes the median. Note 9 indicates that a flat median can be used if the depressed median is not feasible. This would require the designer to provide drainage for the median especially if the roadway has a break in slope (see note 17). A modification will be made to the drawing to call attention to the requirement of the median slope to extend 1 foot below the pavement thickness layer.		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number	DD 11A	Sheet 3	of	7
Date:	April 2008	Facilitator:	Patrick Cowley	

7	David Schwartz, R2 Resident Engineer	DD 11A	Grade separated is not the right term. I think you are going for Depressed median or unpaved median. Grade separated gives the impression of differing elevations for one direction from the other.		
			Response: The intent of the drawing is exactly as you stated above in that we want an arterial standard that has "differing elevations for one direction from the other." This roadway type is not anticipated to have intersections, but to function like a lower speed freeway. The median is not the focus of the drawing.		

8	David Schwartz, R2 Resident Engineer	DD 11A	I may be wrong but if this is an arterial that probably means fairly heavy traffic and I can not see a time when GB would not be used.		
			Response: The note will be revised to reflect the use of granular borrow.		

9	David Schwartz, R2 Resident Engineer	DD 11A	If a min pavement of 6" HMA 6" of UTBC and 12" of GB is use at a slope of 6:1 you would have to have a width from shld to shld of 44' That is just 6' from the freeway standard and would rarely occur. The stated 14' min. Would never occur.		
			Response: The labels "Divided Median" and "Undivided Median" have been removed to avoid confusion. It is the responsibility of the designer to calculate the needed width to adequately provide a depressed median given the roadway conditions (pavement thickness, etc.) A 14' median could occur with an undivided median.		

8	David Schwartz, R2 Resident Engineer	DD 11A	NOTE #14 & #15 Algebraic diff is not want you want used. A ditch could be formed using this. It needs to still run off the pavement and not down the pavement.		
			Response: This language refers to the absolute value of a negative grade break and is similar on other standard drawings and in the AASHTO Green book (p.307).		

9	David Schwartz, R2 Resident Engineer	DD 11A	NOTE #13 Instead of directing that this must happen maybe advise that it can happen.		
			Response: The note has been changed to reflect the fact that slope breaks are not mandatory.		

10	David Schwartz, R2 Resident Engineer	DD 11A	NOTE #16 The term positive separation I believe means a barrier of some sort such as cable barrier, concrete barrier or a curb. I would show a cable barrier and have a note that if the median is wider than 50' the cable barrier may be eliminated. Also why 50 min. The max clear Zone for 60 MPH @ 6:1 is 32' with down to 16' being acceptable depending on the ADT.		
			Response: Positive separation does refer to a barrier system. A barrier will be shown on the drawings to draw attention to the note. The 50 ft. came from a recent study done for Legacy parkway.		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number		DD 11A		Sheet 4	of	7
Date:		April 2008		Facilitator:		Patrick Cowley
11	David Schwartz, R2 Resident Engineer	DD 11A	Aren't there only 3 acceptable positive separation methods at these speeds. Concrete barrier, Cable Barrier, and guardrail. I would just list them at this point.			
			Response: At this time there are three alternatives, but listing these items in a standard does not allow for future alternatives to be considered without a design exception.			
12	Scott Andrus, R3 Construction Engineer	DD 11A	The only concern I noted on this drawing is the inside shoulder width on an undivided median, I think the 6 ft allowed in note 18 should be 8 ft, we've experienced a lot of difficulty with shoulders too narrow to allow maintenance vehicles to access on our section of I-15 from about the Alpine interchange to the University Parkway. The barrier collects any debris and garbage requiring frequent clean-up which is difficult with insufficient width for vehicles as well as regular activities such as sign maintenance. Other than that I had no concerns.			
			Response: AASHTO allows for a four foot shoulder with a two foot barrier offset. This drawing is a reflection of the aggressive minimalistic approach to roadway design.			
13	Doug Bassett, R3 Traffic Engineer	DD 11A	No comments.			
			Response:			
14	Tyler Yorgason, Standards Representative, ACEC	DD 11A	In the typical section at the top of the page, a 14' MIN is specified under the note "DEPRESS MEDIAN WHEN FEASIBLE". A couple of things should be noted: a) In addition to Note 16 that is already referred to on the drawing, Note 9 should probably be referenced, or maybe combined with Note 16. b) Based on Note 9, it appears that much more than 14' width would be needed for a depressed median, depending on pavement thickness and which type of "positive separation" is used. With a pavement section thickness of only 18", a minimum width of 38' would be required. Maybe note 19 can be combined into note 16, if desired.			
			Response: For a): Note 9 will be referenced for greater clarity b) The "10:1 slope or flatter" mentioned in Note 9 does not depend on the pavement thickness. The 14' minimum median width is based on AASHTO requirements.			
15	Tyler Yorgason, Standards Representative, ACEC	DD 11A	Though required per Note 9, the drawing doesn't show a minimum depth of 1' below the pavement thickness.			
			Response: The drawing will be updated to show the minimum depth.			

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

Review Comments

Std Dwg/Spec Number	DD 11A	Sheet 5	of	7
Date:	April 2008	Facilitator:	Patrick Cowley	

16	Tyler Yorgason, Standards Representative, ACEC	DD 11A	In Note 4, the second and third sentences may conflict; please clarify which sentence receives priority. Specifically, is the 1' Min below the GB layer required (as stated in Note 4 and in the leaded note on the Divided Median section) at all times, or only when "other measures to drain all pavement thickness layers" are not provided.		
			Response: The intent of the note is to clarify the need for proper drainage of the pavement thickness. In most cases, a cut ditch is used, so Note 4 gives further clarification for the depth needed for the cut ditch.		

17	Tyler Yorgason, Standards Representative, ACEC	DD 11A	The Plan Sheet Development Standards on UDOT's web site shows that the tick mark (') indicating feet in the dimensions should not be shown on typical sections in plan sets. Is there any plan to revise or update standard drawings to maintain consistency?		
			Response: The comment is noted. We will monitor and update as changes are needed.		

18	Tyler Yorgason, Standards Representative, ACEC	DD 11A	My ignorance is showing all too readily here, but can you clarify the symbol in the bottom of the median?		
			Response: The symbol was found to have no significance, and will be removed.		

19	Tyler Yorgason, Standards Representative, ACEC	DD 11A	Note 2 indicates that the "clear zone may extend into cut or fill slopes." Note 4 may contradict by requiring a constant slope from the edge of pavement to the outer edge of the clear zone for fill conditions. Does this just mean that the 6:1 slope may be required (as is shown on the drawing) to continue beyond the bottom of the granular borrow into the "fill" before the steeper (4:1 or steeper) fill slope can be constructed? Maybe the last sentence of Note 2 could be modified to read something like "When allowed by the Roadside Design Guide, clear zone may extend beyond cut ditch."		
			Response: Note 4 will be modified to allow for clear zone compliant slopes to be constructed in lieu of a constant slope to the edge of the clear zone.		

20	Tyler Yorgason, Standards Representative, ACEC	DD 11A	Referring to the cut slopes noted on the right side of the Divided Median section, if bedrock is intended to be included in all cuts up to 5' in height (30' width), the bottom note should probably read "6:1 FOR CUTS UP TO 5'". Otherwise, delete "(INCLUDING BEDROCK)".		
			Response: "SOIL" has been removed from the note.		

21	Tyler Yorgason, Standards Representative, ACEC	DD 11A	Are there any cases this could be used for speeds greater than the noted 60 mph? Are these design speeds or posted speeds?		
			Response: This drawing is intended for design speeds between 50 to 60 mph. This will be noted on the drawing.		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet

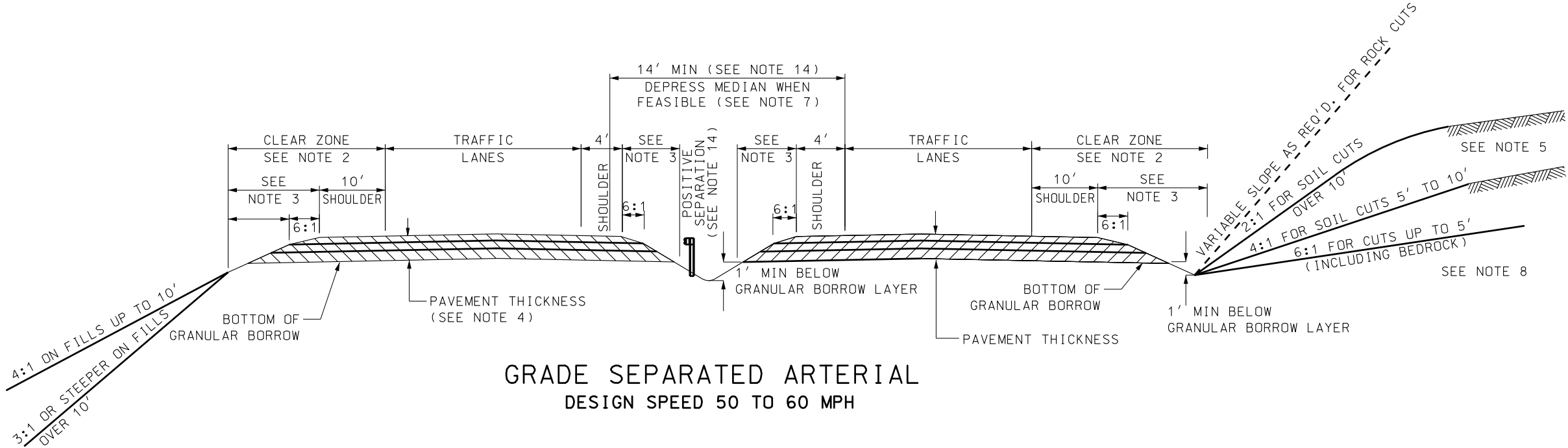
Review Comments

Std Dwg/Spec Number		DD 11A		Sheet 6	of	7
Date:		April 2008		Facilitator:		Patrick Cowley
22	Tyler Yorgason, Standards Representative, ACEC	DD 11A	Is this different enough from DD 11 to have a separate drawing? Can the Undivided Median section and some changes to the notes just be added to that drawing?			
			Response: We attempted to combine the drawings because of their similarities. However, it eliminates confusion created by the intersection detail on DD 11.			
23	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	Show minimum width for cable barrier on divided median drawing.			
			Response: Undivided and divided median labels have been removed. Notes and minimums are applicable for both drawings.			
24	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	Address median drainage concerns with the divided median drawing.			
			Response: The drawing has been modified to address drainage concerns.			
25	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	Add "SEE NOTE 6" below pavement thickness note.			
			Response: Comment will be added.			
26	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	Notes 2 and 4 seemingly contradict each other indicating that there may be cut or fill slopes in the clear zone and that the bottom of the cut ditch must be 1 foot below the bottom of the granular borrow layer,			
			Response: The roadside design guide allows the designer to provide clear zone distances greater than those listed in the table under certain circumstances. These notes provide for this possible occasion.			
27	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	Add "See slope rounding detail in MOI" to note 7.			
			Response: Wording will be added to Note 7.			
28	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	Add the barrier offset dimension to the undivided median drawing.			
			Response: Dimension will be added and Note 18 updated.			
29	Paul Egbert, R1 Utilities & Environmental Engineer	DD 11A	What is the purpose of Note 17?			
			Response: Note 17 indicates that drainage must be provided in the median if it is paved and the roadway has a break in slope, diverting water to the median.			
30	Bill Lawrence, R2 Preconstruction Engineer	DD 11A	No additional comments.			
			Response:			

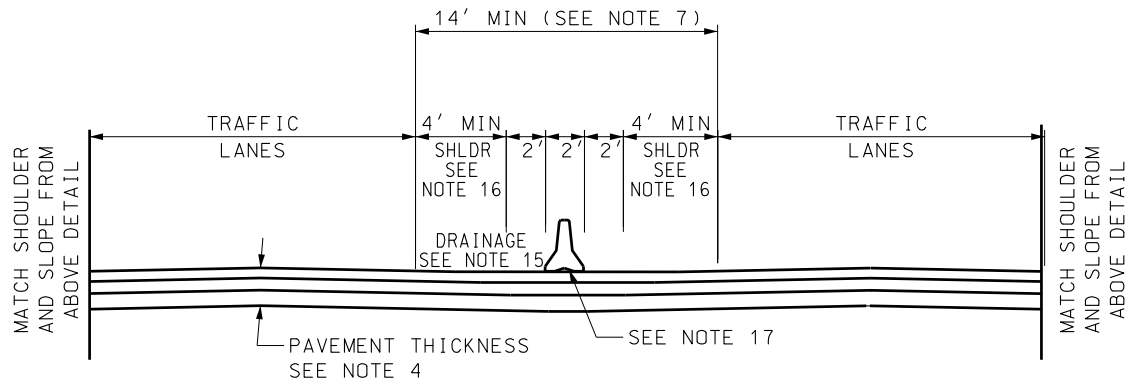
Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate

Standard Drawing/Specification Review Sheet			Review Comments		
Std Dwg/Spec Number		DD 11A		Sheet 7	of 7
Date:	April 2008			Facilitator:	Patrick Cowley
31	Brent Schvaneveldt, R3 Preconstruction Engineer	DD 11A	No additional comments		
			Response:		

Action Code	A	B	C	D
	Submitter will Comply	Submitter to Evaluate	Delete Comment	Others to Evaluate



GRADE SEPARATED ARTERIAL
DESIGN SPEED 50 TO 60 MPH



NOTES:

1. USE THE CURRENT EDITION OF AASHTO: A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS FOR DESIGN OF ROADWAY ELEMENTS NOT SHOWN ON THIS STANDARD DRAWING.
2. USE THE CURRENT EDITION OF AASHTO ROADSIDE DESIGN GUIDE FOR CLEAR ZONE REQUIREMENTS. CLEAR ZONE MAY EXTEND INTO CUT OR FILL SLOPES.
3. MAINTAIN A 6:1 SLOPE FROM TOP OF PAVEMENT TO TOP OF UTBC. MAINTAIN CLEAR ZONE COMPLIANT SLOPES FROM THE TOP OF THE UTBC TO THE OUTER EDGE OF THE CLEAR ZONE IN FILL CONDITIONS. MAINTAIN A CONSTANT SLOPE FROM THE TOP OF THE UTBC TO THE BOTTOM OF THE GRANULAR BORROW LAYER OR PROVIDE OTHER MEASURES TO DRAIN ALL PAVEMENT THICKNESS LAYERS IN CUT CONDITIONS. MAINTAIN A MINIMUM OF ONE FOOT VERTICAL DISTANCE FROM THE BOTTOM OF THE GRANULAR BORROW LAYER TO THE BOTTOM OF THE CUT DITCH. THERE MAY BE CUT FORESLOPES AND BACKSLOPES IN THE CLEAR ZONE.
4. PAVEMENT THICKNESS CONSISTS OF HARD SURFACING, UTBC, AND GRANULAR BORROW (IF USED).
5. INSTALL SURFACE DITCH (OPTIONAL) WHEN SHEET FLOW DRAINAGE IS TOWARDS CUT SLOPE. DRAIN SURFACE DITCH TO NATURAL DRAINAGE OR ROADSIDE DITCH. PROVIDE OTHER MEASURES TO PREVENT ERODING CUT SLOPES IF SURFACE DITCH IS OMITTED. SEE STD DWG DD 2 FOR DETAILS. ALSO SEE SLOPE ROUNDING DETAILS IN ROADWAY DESIGN MANUAL OF INSTRUCTION.
6. SEE STD DWG DD 4 FOR TYPICAL DETAILS FOR SECTION ON CURVE AND SECTION ON TANGENT. SEE STD DWG DD 2 FOR TYPICAL SECTION ON DITCH FLARING AND BENCHED SLOPE.
7. USE FLAT PAVED MEDIAN (10:1 OR FLATTER) WHERE MEDIAN IS NOT OF SUFFICIENT WIDTH TO PROVIDE A DEPTH OF 1 FOOT BELOW THE PAVEMENT THICKNESS.
8. THE SLOPES SHOWN FOR CUT AND FILL HEIGHTS ARE SUGGESTED VALUES. SLOPES MAY DEVIATE FROM THESE SUGGESTED VALUES TO MEET PROJECT SPECIFIC REQUIREMENTS.
9. RANGE OF SUPERELEVATION IS THE PAVED WIDTH.
10. USE 2% MINIMUM CROSS SLOPES.
11. PLACE ADVERSE SLOPE BREAKS AT SHOULDER OR LANE LINES IF APPLICABLE.
12. USE 6% MAXIMUM ALGEBRAIC DIFFERENCE FOR SLOPE BREAKS BETWEEN SHOULDER AND LANE LINES.
13. USE 4% MAXIMUM ALGEBRAIC DIFFERENCE FOR SLOPE BREAKS BETWEEN LANE LINES.
14. POSITIVE SEPARATION IS REQUIRED FOR MEDIAN WIDTHS LESS THAN 50'.
15. PROVIDE UNDERGROUND DRAINAGE AT PAVED MEDIAN IF ROADWAYS HAVE A BREAK IN SLOPE THAT DIVERTS WATER TO THE MEDIAN.
16. USE 4' SHOULDERS FOR UP TO TWO TRAFFIC LANES IN EACH DIRECTION. USE 8' SHOULDERS FOR THREE OR MORE TRAFFIC LANES.
17. USE ANY ACCEPTABLE POSITIVE SEPARATION.

REVISIONS			
NO.	DATE	APPR.	REMARKS
1	04-24-08	RM	NEW DRAWING.

UTAH DEPARTMENT OF TRANSPORTATION			
STANDARD DRAWINGS FOR ROAD AND BRIDGE CONSTRUCTION			
SALT LAKE CITY, UTAH			
RECOMMENDED FOR APPROVAL		APR. 24, 2008	DATE
CHAIRMAN STANDARDS COMMITTEE		APR. 24, 2008	DATE
APPROVED			
DEPUTY DIRECTOR			

GRADE SEPARATED ARTERIALS OTHER THAN FREEWAYS	
STANDARD DRAWING TITLE	
STD DWG	DD 11A

Action Item Update for April 24, 2008 Standards Committee Meeting

Item 1, Supplemental Specification 01554M, Traffic Control: Item is on the agenda for the April 24, 2008 meeting for approval.

Item 2, SW Standard Drawings, cracking issue. Richard Miller is now the contact for this. He was just contacted for an update so there is no report at this time.

End of Agenda Package